

ARTESIA CORRIDOR

SPECIFIC PLAN



JULY 11, 2006





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ORD. NO. 1679

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1. SUMMARY STATEMENT

1.1 OVERVIEW

Located along one of the City's primary arterial corridors, the Artesia Corridor Specific Plan area provides a prime opportunity to create a new community gateway into the City and to establish a new node of activity that will bring vitality to the surrounding area. Historically, Artesia Boulevard has served as one of Gardena's major east-west thoroughfares in the southern end of the City. Comprised of predominantly modest commercial and light industrial uses lined by limited streetscape treatments, this portion of Artesia Boulevard has traditionally been a "pass-through" street, with the majority of the traffic traveling through the area to another destination in a neighboring city.

As the City's population continues to grow, it will become increasingly important to make sure undeveloped and underutilized properties implement techniques that encourage the most efficient use of land with each new development proposed. According to the 2006 General Plan, the City of Gardena is almost completely built out, with approximately 44 acres of vacant land remaining. The Artesia Corridor Specific Plan contains approximately 15 acres of vacant land—nearly a third of the total remaining land within the City. Moreover, the combined acreage and depth of these parcels represent a critical mass of property that can support the development of a large project. It is therefore critical that the City establish clear direction for the location, type and intensity of development in this area.

The Artesia Corridor Specific Plan provides a new vision for Artesia Boulevard between Western and Normandie Avenues, adjacent to the Dominguez Channel. The City created the Specific Plan to transform this 44-acre area into a special place for the residents and property owners of Gardena. What is now primarily a collection of unassuming businesses and underutilized parcels can grow into an oasis of mixed-use activity. The provisions of this Specific Plan will foster the creation of a recognizable, landmark development for Gardena that appeals to the City's diverse population, stands out from other development along Artesia Boulevard, and provides much-needed gathering spaces for the community.



Since the City does not have a redevelopment agency, it was important that this Specific Plan be structured in a way that would motivate private development interests to redevelop the site. Redevelopment of the area demands a balance of land uses to retain the City's commercial base and increase valuable sales-tax dollars while providing much-needed upscale housing that could serve as "move-up" housing for existing residents or attract new young professionals. A key objective of the Specific Plan includes the successful introduction and integration of new retail and residential uses into the Specific Plan project area to complement and enhance existing uses such as Marukai, which has been successful in drawing customers from the surrounding subregion. By creating a "mini destination" at the Artesia Corridor Specific Plan site where patrons can experience a unique and desirable shopping environment, the project can help reduce sales leakage to adjacent communities.

The Specific Plan provides up to 375,000 square feet of general commercial and 40,000 square feet of restaurant along Artesia Boulevard, one of the most important commercial corridors in the City of Gardena. These uses will offer local residents enhanced shopping, eating and entertainment opportunities while



enhancing the City's revenue generation. To realize a project that goes beyond a typical power center, the Specific Plan creates a unique mixed-use environment with a combination of commercial, residential, and live/work uses oriented around a centralized main street. Up to 300 residential units, including 70 live/work units, are allowed within the center of the project area, with 300 to 500 square feet of office use per live/work unit. The high-density residential and live/work uses respond to the growing demand for housing by young professionals seeking an urban lifestyle and at-home business opportunities. The main street functions as the project's core circulatory system and provides vehicular and pedestrian connections between uses to create a cohesive, enjoyable and walkable environment.

1.2 SPECIFIC PLAN OBJECTIVES

The overall purpose of this Specific Plan is to generate momentum and provide comprehensive direction for the improvement of the Artesia Corridor project site, while implementing the goals and policies of the City's General Plan. The project is also guided by the following major objectives:

1. Establish a unified vision for the project site, replacing the existing patchwork of development with a cohesive, complementary mix of uses structured around a comprehensive set of circulation and infrastructure systems.
2. Create a successful mixed-use development that maximizes the commercial potential of the project site.
3. Incorporate a variety of commercial and retail opportunities, such as shopping, and entertainment activities to serve the population and increase revenues to the City of Gardena.
4. Apply innovative planning and design solutions to create a sense of place at multiple scales.
5. Facilitate lot consolidation and redevelop the site as a cohesive area to prevent piecemeal development and increase property values of the existing, underutilized commercial and industrial properties.
6. Provide new housing opportunities for the community, encouraging high quality, high density residential units that appeal to existing residents looking for "move-up" housing within the City and to young professionals seeking shorter commutes to jobs, restaurants, and entertainment opportunities.
7. Take advantage of the area's location and exposure by establishing a welcoming gateway to the City characterized by distinct and attractive signage, architecture, and landscaping, both on-site and in the public right-of-way.
8. Implement a main street concept that optimizes circulation for both vehicular and pedestrian traffic, internalizes pedestrian activity to buffer it from the vehicular traffic along Artesia Boulevard, and establishes connectivity between uses infused with pedestrian friendly and walkable spaces.
9. Develop an existing underutilized brownfield commercial and industrial site into a well designed and attractive mixed-use development.



2. INTRODUCTION

2.1 AUTHORITY

The City of Gardena initiated and prepared the Artesia Corridor Specific Plan pursuant to the provisions of California Government Code, Title 7, Division 1, Chapter 3, Article 8 (Sections 65450 through 65457). The law allows for the preparation of specific plans as may be required for the implementation of the general plan. Specific plans act as a bridge between the general plan and individual development proposals. Specific plans combine development standards and guidelines, capital improvement programs, and financing methods into a single document, which is tailored to meet the needs of a specific area. Jurisdictions may adopt specific plans by resolution or ordinance.

Upon adoption, this Specific Plan will serve as zoning for the properties involved. It establishes the necessary plans, development standards, regulations, infrastructure requirements, design guidelines, and implementation programs on which subsequent project-related development activities are to be founded. It is intended that local public works projects, design review plans, detailed site plans, grading permits and building permits, or any other action requiring ministerial or discretionary approval applicable to this area be consistent with this Specific Plan.

2.2 PURPOSE AND INTENT

The Artesia Corridor Specific Plan provides a new vision for Artesia Boulevard between Western and Normandie Avenues, adjacent to the Dominguez Channel. The City created the Specific Plan to transform this 44-acre area into a special place for the residents and property owners of Gardena. What is now primarily a collection of unassuming businesses and underutilized parcels can grow into an oasis of mixed-use activity. The provisions of this Specific Plan will foster the creation of a recognizable, landmark development that appeals to the City's diverse population, stands out from other development along Artesia Boulevard, and provides much-needed gathering spaces for the community.

The creation of a Specific Plan establishes a unified vision for the project site with the goal of applying innovative planning and design solutions to create a sense of place at all scales. The Specific Plan will create land uses, circulation design, site development standards, and design guidelines that promote the site's long term success and enhance the character of the site and adjacent land uses. This document will provide for a scale and mix of uses for the most efficient use of the existing parcels, taking into consideration size and shape, proximity to existing and adjacent uses, and the potential for incremental development on the site.

The Specific Plan provides for the development of up to 375,000 square feet of general commercial, 40,000 square feet of restaurant, and up to 300 residential units, including 70 live/work units. Each live/work unit would contain approximately 300 to 500 square feet of office use.

2.3 SITE LOCATION

2.3.1 Regional Setting

The City of Gardena is located in the South Bay area of Los Angeles County, approximately 10 miles south of downtown Los Angeles, approximately 7 miles southeast of the Los Angeles International Airport, and approximately 12 miles north of the Los Angeles Ports in San Pedro. The location of the project site in its regional setting is illustrated in Figure 2.3a. A number of freeways provide regional access to and from Gardena, including the San Diego Freeway (I-405), the Artesia Freeway (SR-91), the Harbor Freeway (I-110), and the Century Freeway (I-105). Since its incorporation in 1930, the City has grown from a 3-



square-mile community known for its farm products to a 5.9-square-mile, highly developed City with a population of over 61,000.

2.3.2 Local Setting

The Artesia Corridor project represents 44 acres of land in the southern portion of the City, bounded by Artesia Boulevard (SR-91) to the north, Western Avenue to the west, Normandie Avenue to the east, and the Dominguez Channel to the south. The location of the project site within the City boundaries is illustrated in Figure 2.3a. The site contains a variety of occupied and unoccupied commercial and industrial uses, one occupied residence, three unoccupied residences, and several vacant parcels. The eastern portion of the site includes the Union Pacific Railway tracks. The southern edge of the site includes the Dominguez Flood Control Channel and the Los Angeles County Flood Control District rights-of-way, which are adjacent to both sides of the Dominguez Channel. The Dominguez Channel extends from the Los Angeles International Airport to the Los Angeles Harbor and drains large, if not all, portions of the Cities of Gardena, Inglewood, Hawthorne, El Segundo, Lawndale, Redondo Beach, Torrance, Carson and Los Angeles.

An aerial photograph of the project site vicinity is provided in Figure 2.3b. Local land uses in the immediate vicinity of the site include commercial uses on the north side of Artesia Boulevard east of Western Avenue and west of Normandie Avenue, multi family residential uses on the north side of Artesia Boulevard between the above-mentioned commercial uses, with single and multi family residential uses north of all the uses along Artesia Boulevard.

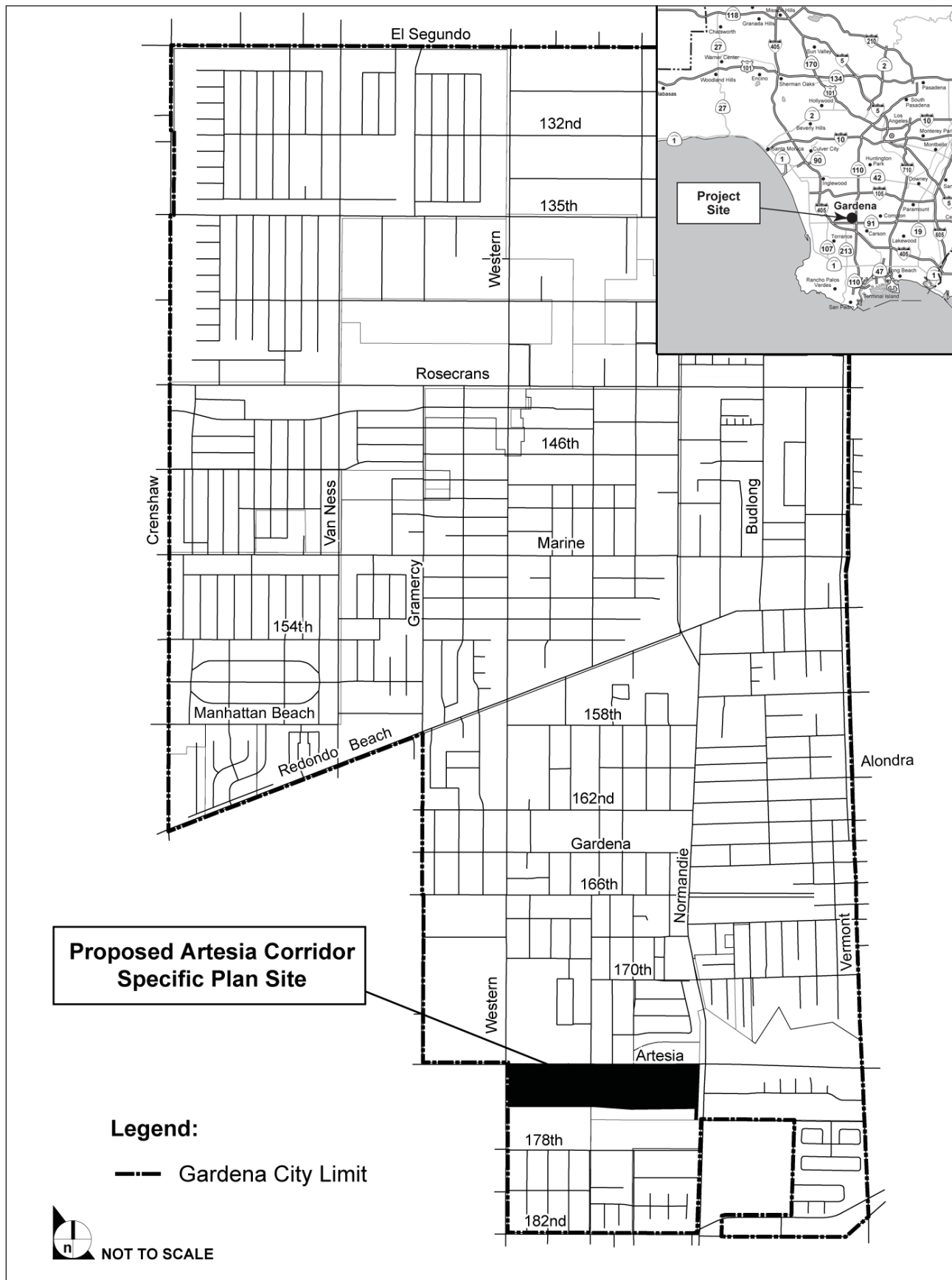
Commercial uses are located west of the site along the west side of Western Avenue, south of Artesia Boulevard. South of the project site, within the Southern California Edison (SCE) right-of-way, are located nursery uses, with equestrian uses south of the eastern portion of the site. 178th Street runs east–west, south of the SCE right-of-way between Western and Normandie Avenues. South of the right-of-way and north of 178th Street is a mobile home park fronting on Artesia Boulevard, and industrial uses are located between the mobile home park and Normandie Avenue.

The east side of Normandie Avenue has commercial uses along Artesia Boulevard, with single-family homes south of this and north of the Dominguez Flood Control Channel and SCE right-of-way. Gardena High School is located on the east side of Normandie Avenue, south of 177th Street.

2.4 PROJECT BACKGROUND

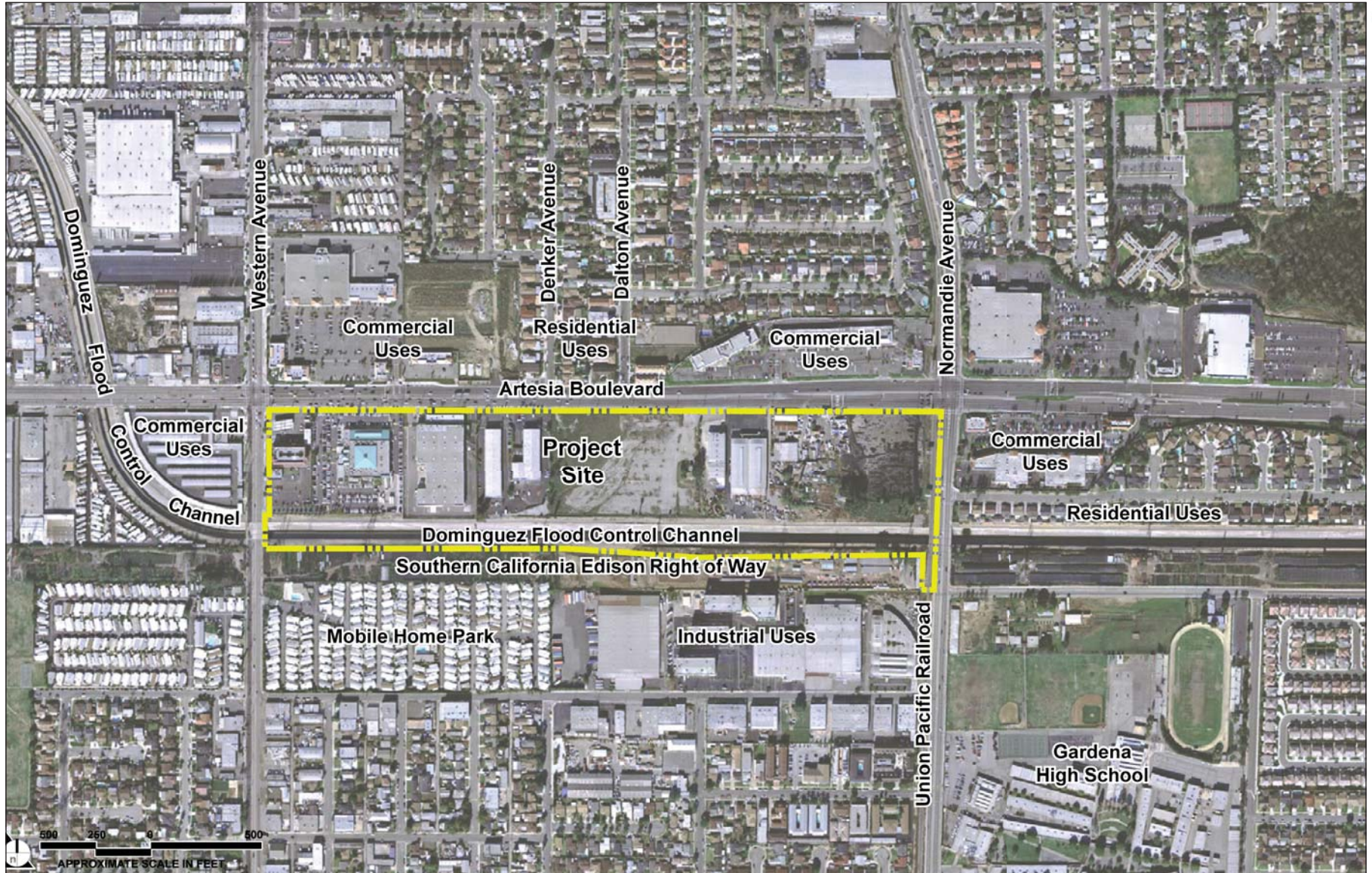
A citywide retail analysis, completed in December 2004 for the City of Gardena, examined retail opportunities within and surrounding the City and summarized the opportunity for retail development at distinct locations within the City. The report found that several underutilized properties along Artesia Boulevard, between Western and Normandie Avenues, offered a prime location for commercial development—particularly if the properties were consolidated to create a large enough area in which to concentrate retail efforts and attract quality commercial uses.

The City subsequently changed the General Plan land use designation for the majority of these properties from Industrial to General Commercial during the update of the General Plan in 2006. The City also adopted a policy to consider the adoption of a specific plan for this area (referred to as the Artesia Corridor), to promote and direct revitalization efforts and to evaluate the potential of a mix of commercial and residential uses. In response, the City has organized the Artesia Corridor Specific Plan to create a unique mixed-use development that maximizes the retail potential of the Corridor, attracts high quality residential development, and creates a special place for the City of Gardena.



source: Impact Sciences, March 2006

Figure 2.3a Regional and Project Vicinity Map



source: Impact Sciences, 2006

Figure 2.3b Project Aerial



2.5 ENVIRONMENTAL ASSESSMENT

The Environmental Impact Report (EIR) serves primarily as a source of environmental information for the City of Gardena as lead agency for the project. The EIR describes the potential impacts that could result from the adoption of the Artesia Corridor Specific Plan. Subsequent development projects within the Artesia Corridor Specific Plan are anticipated. While the EIR has been prepared as a project EIR, as defined by §15161 of the CEQA Guidelines, subsequent projects that are within the scope of this EIR may be subject to a more limited environmental review process if that is determined to be necessary by the Director of Community Development of the City of Gardena.



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3. CONTEXT AND CONDITIONS

3.1 EXISTING SITE CONDITIONS

3.1.1 Property Configuration and Existing Uses

The Artesia Corridor Specific Plan project site is currently divided into 21 parcels, ranging in size from 0.2 acres to 7.3 acres. The majority of parcels are owned by separate individuals, with a small number owned by the Los Angeles County Flood Control District or the Union Pacific Railway. Some of the properties are landlocked and must obtain access to Artesia Boulevard through access easements. As of 2006, the site contained commercial, industrial, and residential uses, including such uses as a gas station, hotel, market, restaurant, auto service center, garment factory, and residential homes. Six parcels are completely or partially vacant or unoccupied. A map of the existing parcels and uses is provided in Figure 3.1a.

3.1.2 Circulation

The major freeways that provide regional access to the project site include I-405 to the north, and I-110 and SR-91 to the east. Circulation along I-405 connects to the project via Western and Normandie Avenue, which run along the eastern and western edges of the project boundary respectively. Circulation along I-110 reaches the project via SR-91, which transitions into Artesia Boulevard at that intersection and runs along the northern boundary of the site.

The three major streets bordering the project, Western Avenue, Normandie Avenue, and Artesia Boulevard, provide local access to the site. Artesia Boulevard is a major divided arterial, with a total of six lanes traversing along the northern edge of the site. Western Avenue is a minor arterial with four lanes of travel, and runs along the western boundary of the project. Normandie Avenue is a collector street, with a total of four lanes of travel, and runs adjacent to the Union Pacific Railway tracks along the eastern edge of the site.

3.1.3 Topography and Hydrology

The topography of the project site is generally level, with areas that are gently to moderately sloping. Artificial fill soils cover most of the site and are thickest near the center of the site where the Dominguez Creek was infilled, and at the eastern end of the site where a sump was excavated and filled. The site is located in the Coastal Plain West Coast groundwater basin and, based on well records at the Los Angeles County Department of Public Works, depth to groundwater is approximately 85 feet below ground surface. However, groundwater was encountered at depths ranging from 20 to 24 feet below ground surface during a site investigation undertaken in support of the Project's EIR in November 2005. The direction of groundwater flow is generally to the west-southwest.

3.1.4 Geology and Soils

As are all of the jurisdictions within southern California, the City of Gardena is located within a seismically active region that includes numerous active, potentially active, and inactive fault traces and, consequently, is subject to the risks and hazards associated with potentially destructive earthquakes. The area located along Artesia Boulevard and the Dominguez Flood Control Channel in the southern portion of the City is located within a liquefaction zone. Liquefaction is caused by a shock or strain from an earthquake, and involves the sudden loss of soil strength and cohesion and the temporary transformation of soil into a fluid mass. Therefore, older structures within the liquefaction areas of the City that were not constructed or reinforced to meet earthquake standards are vulnerable to structural damage.



Existing Uses (as of 5/2006)

Existing Parcel Boundaries

| | | | | | |
|---|--------------------------------|----|----------------------------|----|-----------------------------------|
| 1 | Chevron (0.7 ac) | 9 | Farwest Corrosion (2.1 ac) | 17 | Union Pacific Railway (0.2 ac) |
| 2 | Dynasty Hotel (0.4 ac) | 10 | U-Haul (2.5 ac) | 18 | Union Pacific Railway (0.9 ac) |
| 3 | Wendy's (0.8 ac) | 11 | House (0.2 ac) | 19 | Vacant/Dominguez Channel (3.3 ac) |
| 4 | Marukai (4.6 ac) | 12 | House (0.2 ac) | 20 | Dominguez Channel (2.4 ac) |
| 5 | Apollo Embroidery (4.2 ac) | 13 | Vacant (0.2 ac) | 21 | Dominguez Channel (3.9 ac) |
| 6 | Gardena Auto Center (3.4 ac) | 14 | House (0.2 ac) | | |
| 7 | Vacant (7.3 ac) | 15 | Vacant (5.9 ac) | | |
| 8 | Golden State Alliance (1.2 ac) | 16 | Vacant (<0.1 ac) | | |

source: The Planning Center, 2006



Figure 3.1a Existing Uses



The project site contains portions of the liquefaction zone described above. The project site is underlain by artificial fill soils over most of the site, with younger alluvium and older alluvium beneath the artificial fill soils. The alluvial soils predominately consist of fine-grained soils, with occasional layers of clayey sand and silty sand.

3.1.5 Biological Resources

The City of Gardena is a fully developed community with less than 2 percent of vacant land. The City does include one biological resource of importance, the Willows Wetland. The Willows Wetland is located north of the commercial development at the northwest corner of Artesia Boulevard and Vermont Avenue, approximately 0.25 mile east of the project site. The Willows Wetland is the last approximately eight acres left of a larger lake that once occupied the area, and is home to two sensitive species, the redshouldered hawk and the yellow warbler. Except for the Willows Wetland, the City contains no rare, threatened, or endangered species, nor any special-status plant and animal species. Wildlife present in the City consists of those known to coexist in areas of human habitation (i.e., mammals, birds and reptiles).

The project site is developed with commercial, industrial, and one existing residential use. Neither rare, threatened, or endangered species, nor any special-status plant or animal species are present on the project site. No riparian habitats, sensitive biological communities or wetlands are present on the project site.

3.2 GENERAL PLAN AND ZONING DESIGNATIONS

The City of Gardena recently updated its General Plan (April 2006). The General Plan Land Use Plan designates the project site as General Commercial, with a maximum floor area ratio (FAR) of 0.5. The General Commercial land use designation provides for a wide range of larger scale commercial uses to serve both the needs of the City and the region. It is intended for commercial uses such as regional retail, automobile dealerships, supermarkets, junior department stores, financial centers, professional offices, restaurants, and other commercial uses oriented to the traveling public. The corresponding zoning designation is General Commercial. The 2006 General Plan, Land Use Policy 3.3, which states:

“Consider the adoption of specific plans for the four corners at Van Ness-Rosecrans, Western-Rosecrans, Rosecrans-Normandie centers and the Artesia corridor to promote and direct revitalization efforts, and to further study and evaluate whether a mix of commercial and residential development is appropriate in these areas.”

With the adoption of the General Plan, the site's existing industrial and low density residential uses are considered legal nonconforming uses. This means that the uses would be allowed to remain, but would be limited with regard to expansion and remodeling opportunities to only those efforts required for public health and safety.

A zone change is submitted as part of the Artesia Corridor Specific Plan. The change requested is from General Commercial (C-3) and General Industrial (M-2) to Artesia Corridor Specific Plan. This change would support the Commercial and Mixed-Use land use categories proposed as part of the Specific Plan and would be supportive of the City's General Plan policy allowing the development of residential uses within the Specific Plan area.

3.3 OPPORTUNITIES AND CONSTRAINTS

A variety of factors must be considered to maximize the development potential of the project. The significant opportunities and constraints that affect the Artesia Corridor Specific Plan relate to the



regional context and location of the project, as well as the site's direct relationship to the adjacent uses, surrounding streets, the channel to the south, and the railroad easement along the eastern edge. Additional issues include the appropriate amount and type of commercial and residential development, the number of property owners on the site, existing circulation patterns, and environmental issues.

3.3.1 Site Accessibility

The project is located adjacent to the City's southern border, in close proximity to the Harbor Freeway (I-110), San Diego Freeway (I-405), and Artesia Freeway (SR-91). The project area benefits from significant exposure to Artesia Boulevard, including two prominent corners at the intersection of Artesia Boulevard and Western and Normandie Avenues. The Specific Plan intends to take advantage of the area's location and exposure by establishing a welcoming gateway to the community on the project site. The gateway will be characterized with distinct signage, architecture, and landscaping to enhance the look and feel of the public right-of-way.



The railroad adjacent to Normandie Avenue requires a compatible land use for that edge of the project area. With the noise from the railroad, and also the vantage point from the intersection, this corner is best suited for commercial uses. However, in providing a minor access point off of Normandie, consideration must be given to the difference in grades when crossing the railroad.

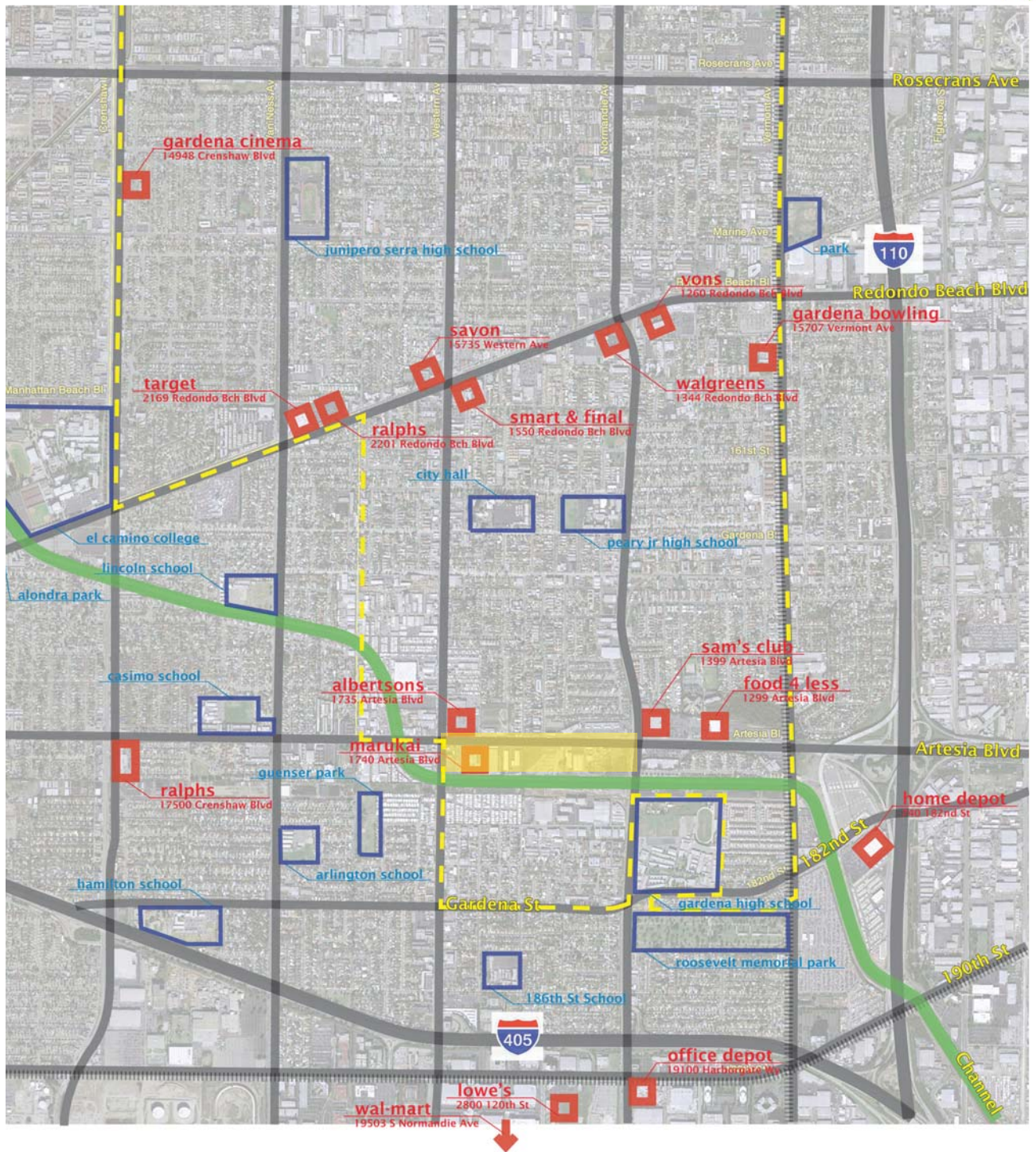
Turning back to Artesia Boulevard, the Artesia Corridor Specific Plan can recognize the advantage of bordering this major street by using gateway signage to attract drivers to the area. Since the

project site lies at the southerly edge of Gardena, the opportunity arises for the project to serve as a gateway to the City. This increases the importance of this stretch of Artesia Boulevard to passing cars, promoting the site as a special place and elevating the image of Gardena.

3.3.2 Land Use

The Specific Plan aims to identify the most appropriate mix of land uses to create a unique, high quality place with successful retail and residential components. A focused retail analysis prepared for the Artesia Corridor site supports a strong potential for commercial development. This, along with the demand for residential, calls for flexibility in accommodating market demands for both commercial and residential uses at this location. The strategic integration and positioning of these different uses contribute to the establishment of a unique destination for residents and visitors.

The retail analysis prepared for the project examined the regional and local context, along with demographics and retail patterns, to determine the commercial potential for the site. The regional context revealed an abundance of significant large retailers and small merchants in the market area. The project area is surrounded by several major retailers located within a mile of the site that have taken advantage of the major corridor frontage opportunities available along nearby arterials. Retailers such as Target, Ralphs, Smart & Final, Vons, Walgreens, Albertsons, Sam's Club and Food for Less are all located within a mile of the project site, making it critical to find the appropriate mix of retailers that can successfully compete for business within the community and will ultimately contribute to a development that can be sustained over a long period of time.



Regional Opportunities & Constraints

- Artesia Specific Plan project area
- Gardena City Boundary
- Significant Retail Locations
- School & Park Locations
- Major Roads
- Channel

source: The Planning Center

Figure 3.3a Regional Opportunities and Constraints



The current lack of medium-sized retailers uncovers the potential for retail tenants ranging in size from approximately 25,000 to 80,000 square feet, as prime candidates for the site. Further analysis identified as possible future tenants specific tenant categories and merchants that currently do not operate stores within the vicinity of the project, summarized below:

| <u>Tenant Category:</u> | <u>Example Tenant:</u> |
|--|--|
| <ul style="list-style-type: none">• General Merchandise• Discount Apparel• Fitness Center• Arts & Crafts• Office Supplies• Pets | <ul style="list-style-type: none">• Kohl's• T.J. Maxx and/or Old Navy• LA Fitness and/or Bally's Total Fitness• JoAnn's• Office Max• PetsMart |

The retail analysis and comments from the public during outreach efforts also indicated a desire for quality sit-down restaurants. Additional elements of the site identified as positive attractors for future retail merchants include visibility along major traffic corridors, synergy with adjacent retailers, adequate developable area for larger retailers, and spacing from existing stores. Site plan design that provides for suitable access points and efficient internal circulation also add to the commercial potential. The market analysis estimates that the Artesia Corridor Specific Plan has the ability to develop at least 300,000 to 350,000 square feet of general commercial and restaurant uses.

The central portion of the site only has frontage to Artesia Boulevard on one side, decreasing the land's value for retailers because of this midblock status. This area may hold a higher potential for residential land uses. The introduction of residential could stimulate further development, appeal to existing residents looking for "move-up" housing within the City, and attract young professionals looking for high quality homes in a mixed-use environment.

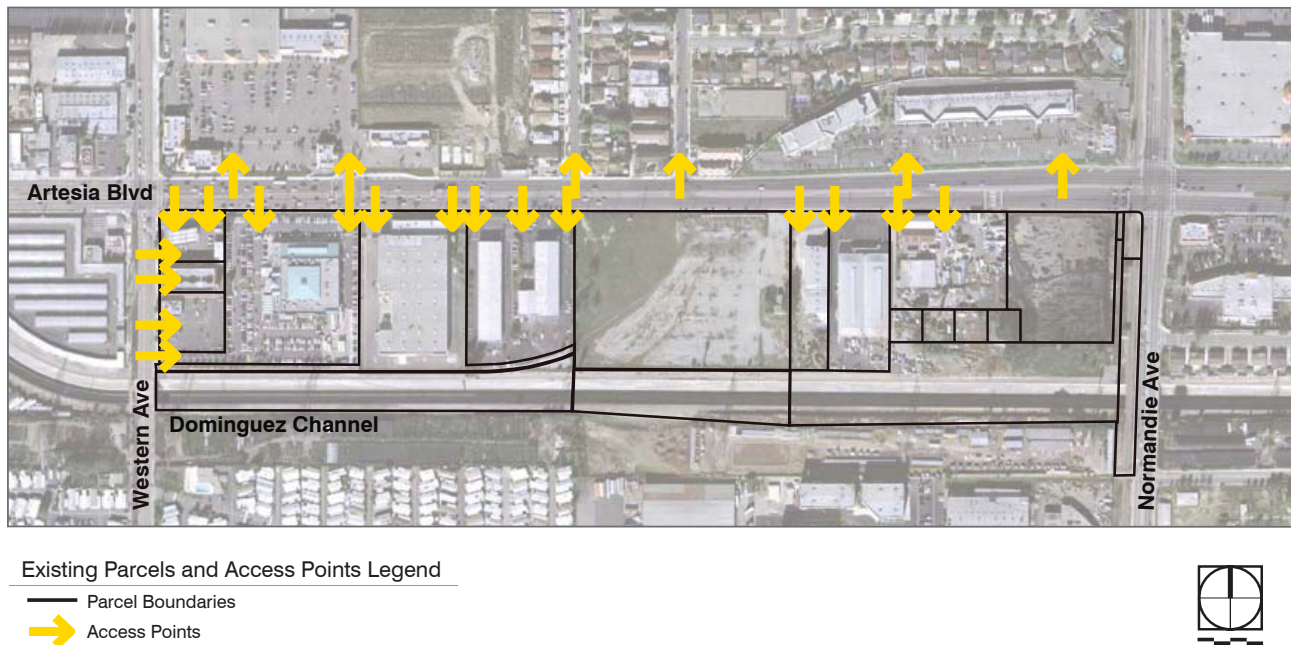
The market research analysis identified a lack of distinctive commercial areas within the City of Gardena. The strategic integration of commercial and residential uses within a site plan that emphasizes the pedestrian lends itself to creation of a unique place that attracts and encourages people to gather and shop. The coordination of these land uses enables the project to take full advantage of the site potential.

3.3.3 Property Ownership

The existing site is comprised of multiple properties owned by several individual parties. One of the concerns in creating a cohesive plan for the site is to recognize these different parcels and find a way to bring them together in a plan that is mutually beneficial to all property owners. The incremental development of individual properties without a unified vision may fail to realize the full potential of the site.

The Specific Plan aims to replace the previous patchwork of development with a cohesive, complementary mix of uses guided by a comprehensive set of circulation and infrastructure systems. The Specific Plan will facilitate lot consolidation to prevent piecemeal development, and redevelop the site as a unified whole to increase property values.

Phasing will also be important to accommodate existing land uses while allowing for other parts of the site to grow and develop in an organized manner.



source: The Planning Center

Figure 3.3b Existing Parcels and Access Points

3.3.4 Circulation

The site is located along a major traffic corridor, as Artesia Boulevard transitions to the Artesia Freeway (SR-91). Additionally, the site can be easily accessible from the 405 and 110 freeways. The site is bounded on three sides by major streets, Western Avenue, Artesia Boulevard, and Normandie Avenue, all experiencing high volumes of traffic. The high volumes of traffic offer a draw for quality commercial uses seeking significant exposure and access. The full commercial potential of the site, however, is limited by the current circulation pattern, in which each business interacts separately with Artesia Boulevard and takes access through one or more driveways.

Even with over one-third of the project site currently vacant, there are currently 17 total access points to the project site, 13 of which are located along the southern side of Artesia Boulevard. Only one access point off of Artesia Boulevard is a signalized intersection. Each one of the unsignalized driveways can act as a point of conflict, as traffic must slow down along Artesia Boulevard to allow vehicles to enter or exit the project site. Additionally, internal access opportunities are unavailable, so travelling from one property to another requires vehicles to re-enter Artesia Boulevard, creating additional traffic and congestion. With one cohesive plan for the Artesia Corridor project, an organized internal circulation system will minimize access points intersecting Artesia Boulevard, creating more efficient traffic patterns for both Artesia Boulevard and the site and enhancing the project's commercial potential. It will also create more pedestrian friendly spaces that are protected from the high volumes of traffic that are carried on the surrounding arterial streets.

3.3.5 Environmental Issues

One of the more important environmental constraints on the site is the contamination of the parcel adjacent to the railroad along Normandie Avenue. Referred to as the Gardena Sumps, these sumps were dumping grounds for waste oil sludge, rinse water, acids, and tank bottom sludge in the 1940s. Fortunately, the contamination left by the oil waste had limited migration into the soil, and groundwater has not been significantly impacted. Partially as a result of this contamination, the Gardena Sumps site



has sat undeveloped for a number of years, despite its prime corner location. The Specific Plan intends to stimulate development around the Sumps to increase property values and development potential to the point where remediating the site (as necessary) is financially feasible.

Prior to any development of the site, mitigation measures must be developed to minimize the potential impacts of the contamination. Depending on the time frame for the cleanup of the site, the development of this part of the plan may still be delayed for a number of years, which will affect the phasing of development within the Specific Plan site.



4. SPECIFIC PLAN CONCEPTS

4.1 SPECIFIC PLAN OBJECTIVES

The overall purpose of this Specific Plan is to generate momentum and provide comprehensive direction for the improvement of the Artesia Corridor project site, while implementing the goals and policies of the City's General Plan. The project is also guided by the following major objectives:

1. Establish a unified vision for the project site, replacing the existing patchwork of development with a cohesive, complementary mix of uses structured around a comprehensive set of circulation and infrastructure systems.
2. Create a successful mixed-use development that maximizes the commercial potential of the project site.
3. Incorporate a variety of commercial and retail opportunities, such as shopping, and entertainment activities to serve the population and increase revenues to the City of Gardena.
4. Apply innovative planning and design solutions to create a sense of place at multiple scales.
5. Facilitate lot consolidation and redevelop the site as a cohesive area to prevent piecemeal development and increase property values of the existing, underutilized commercial and industrial properties.
6. Provide new housing opportunities for the community, encouraging high quality, high density residential units that appeal to existing residents looking for “move-up” housing within the City and to young professionals seeking shorter commutes to jobs, restaurants, and entertainment opportunities.
7. Take advantage of the area's location and exposure by establishing a welcoming gateway to the City characterized by distinct and attractive signage, architecture, and landscaping, both on-site and in the public right-of-way.
8. Implement a main street concept that optimizes circulation for both vehicular and pedestrian traffic, internalizes pedestrian activity to buffer it from the vehicular traffic along Artesia Boulevard, and establishes connectivity between uses infused with pedestrian friendly and walkable spaces.
9. Develop an existing underutilized brownfield commercial and industrial site into a well designed and attractive mixed-use development.

4.2 LAND USE

The Artesia Corridor Specific Plan provides for a potential mix of approximately 415,000 square feet of commercial, retail, restaurant, and entertainment uses; and up to 300 residential units, including 70 live/work units. The Land Use Plan is designed to accommodate these uses through the creation of six planning areas and three land use categories: Commercial (C), Mixed-Use (MU), and Public (P). The planning areas and land use categories allow for a greater variety of land uses and customized development standards. This approach enables the Artesia Corridor Specific Plan to create a truly unique and vibrant location within the City of Gardena, while ensuring compatibility with uses within and adjacent to the site.



4.2.1 Land Use Categories

Below is a brief description of each land use category in the Artesia Corridor Specific Plan and the location of each use by planning area. An illustration of the land use categories is provided in Figure 4.2a.

Commercial (C)

The Commercial category is intended to provide the site's larger, anchor commercial uses that would benefit most from locations at the intersection of Artesia Boulevard and Western and Normandie Avenues. This category includes commercial uses intended to serve a broad population base and offer a wide range of services to both the community and the subregion. Typical uses in this category include larger regional commercial uses (25,000–80,000 square feet) such as promotional retail-type stores, recreation and entertainment uses such as restaurants, and neighborhood retail and service uses. The Commercial category is applied to Planning Areas 1 and 5.

Mixed-Use (MU)

The Mixed-Use category is intended to generate the site's core, permitting vertical or horizontal integration of residential, live/work, and commercial uses around a centralized main street to produce a unique, walkable shopping, working, and living experience. This category allows for, but does not require a mix of uses. Development can consist entirely of either residential or commercial uses, although buildings consisting only of residential uses are not permitted north of the main street. The Mixed-Use category is applied to Planning Areas 2, 3, and 4.

Public (P)

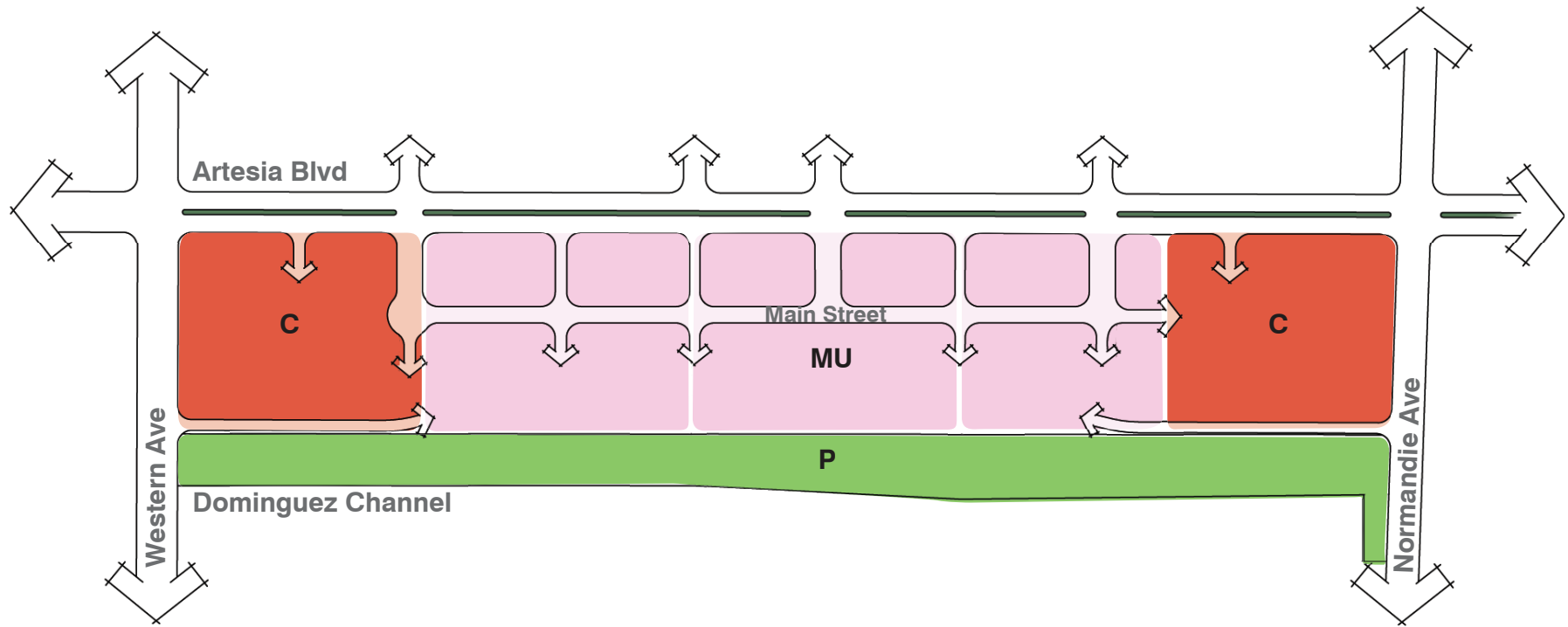
The Public land use category reflects the presence of the Dominguez Channel and utility and railroad easements located at the southern boundary of the project. The long-term intent of the Public designation is to facilitate a connection of the project site with the utility easement (currently owned by Southern California Edison and located outside the Specific Plan) for use as an open space and recreation area. Additional discussion is provided in Section 4.6.

4.2.2 Planning Areas

The Artesia Corridor Specific Plan is divided into six planning areas that identify the location, nature, and potential impacts of development within the site (see Figure 4.2b). The intent, as stated in the planning objectives, is to create a special mixed-use project that embraces the site's commercial potential while introducing high-end, high-density residential and live/work uses around a walkable main street environment.

Although the Specific Plan is a mixed-use project, a minimum amount of 350,000 square feet of commercial development is required throughout the project to ensure that the Specific Plan realizes its commercial potential (provisions for flexibility in the minimum square footage for each planning area are provided in Section 8.2.7).

Additionally, the Specific Plan seeks to encourage lot consolidation and comprehensive planning for cohesive and efficient use of land. The development of the site, though it will likely take place in an incremental fashion, should not take the form of individualized, piecemeal development. The number of property owners and their disparate interests could inhibit the creation of a unified place without careful management by the City. The Specific Plan, therefore, requires that a conceptual master site plan be prepared and approved by the Community Development Director for each planning area prior to any development approval. Each conceptual master site plan must, amongst other requirements,



Land Use Categories

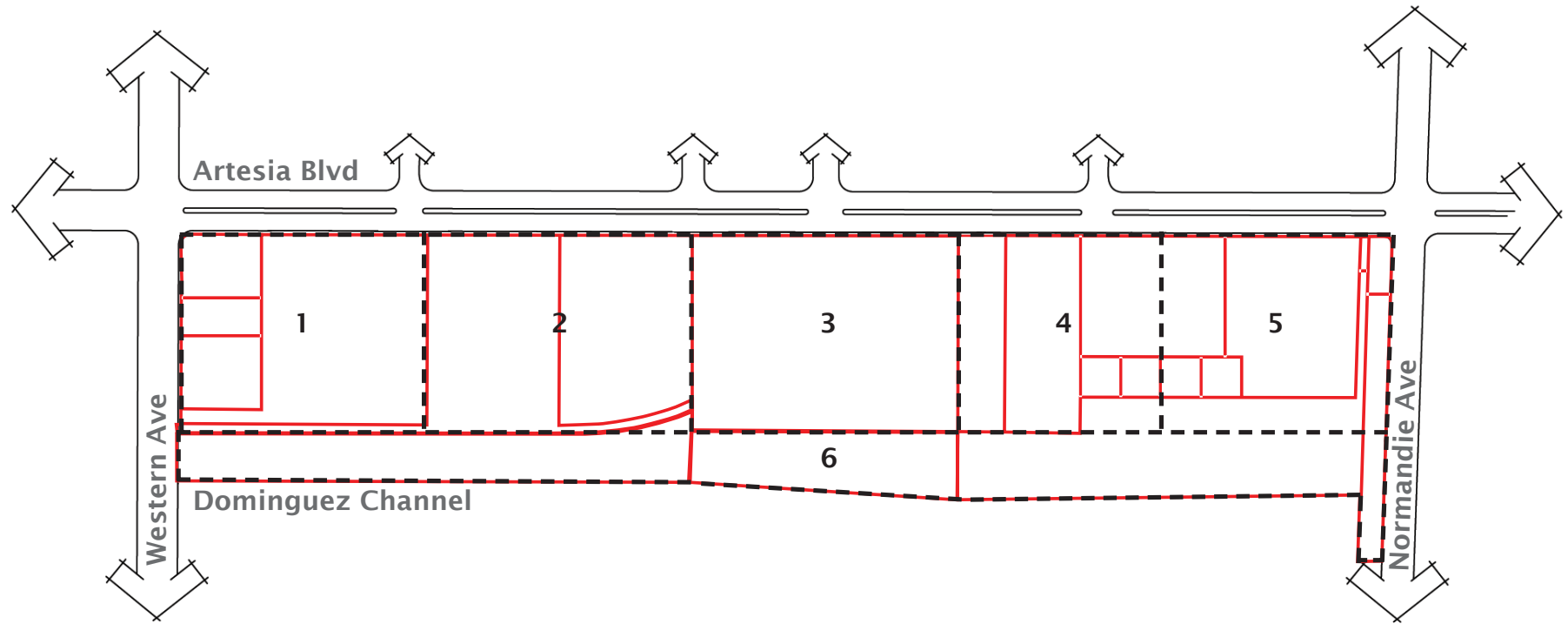
- Commercial (C)
- Mixed Use (MU)
- Public (P)



source: The Planning Center, 2006

Figure 4.2a Land Use Categories





Planning Areas

- Planning Area (1-6)
- Existing Parcels (as of 5/2006)



source: The Planning Center, 2006

Figure 4.2b Planning Areas



demonstrate that the proposed development achieves, helps to achieve, or at least does not inhibit the ability to achieve the targeted minimum commercial square footage in the respective planning area. The procedures and requirements for the conceptual master site plan process are detailed in Section 8.2.2 of this Specific Plan.

The planning areas generally follow existing parcel boundaries to avoid straddling parcels across planning areas. Two parcels, however, fall within both Planning Areas 4 and 5 due to their shape and location. Special coordination efforts between the property owners and City will be necessary to ensure a smooth and equitable planning process.

Planning Area 1 is located at the western edge of the site, situated on the corner of Artesia Boulevard and Western Avenue. Planning Area 5 is located at the eastern edge of the site, situated on the corner of Artesia Boulevard and Normandie Avenue. Planning Area 1 contains an existing successful commercial anchor, while Planning Area 5 is comprised primarily of vacant land and fronts a railroad line along Normandie Avenue. Both Planning Areas 1 and 5 are designated for larger commercial uses due to the significant corner exposure, existing commercial success, and future commercial potential. In recognition of this potential, Planning Area 1 should realize a minimum of 90,000 square feet of commercial development and Planning Area 5 should achieve a minimum of 80,000 square feet of commercial development.

Planning Areas 2, 3, and 4 are located in the center of the site and are classified by the Mixed-Use category to allow for commercial, residential, or live/work uses. Within these planning areas, commercial uses are encouraged along Artesia Boulevard. Residential components are best located in the center or at the rear of the site and should avoid direct frontage along Artesia Boulevard, but a small portion of the live/work uses may be oriented towards Artesia Boulevard subject to the approval of the Community Development Director. Vertical mixed-use elements should be oriented toward internal roadways.

The size and location of the parcels in Planning Areas 2 and 4 invite the greatest amount of mixed-use development. Accordingly, Planning Area 2 (the larger between the two) should realize a minimum of 95,000 square feet of commercial development and Planning Area 4 should achieve a minimum of 70,000 square feet of commercial development. Planning Area 3, as a mid-block parcel, is best positioned for residential development mixed with a minimum of 15,000 square feet of commercial development.

For Planning Areas 2, 3, and 4, residential development is limited to a maximum of 100 units per planning area (live/work, stand-alone residential, or units in a mixed-use building). If a planning area builds out and uses less than the maximum allotted 100 units, the remaining potential may be transferred to the remaining planning areas. The transfer shall be subject to approval by the Community Development Director and is dependent upon interest by future developers of the planning areas.

For example, if Planning Area 3 develops a total of 30 live/work units and 60 stand-alone residential units, the remaining potential of 10 units ($100 - 90 = 10$), may be transferred to endow Planning Area 2 with a potential to develop up to 110 total units. Alternatively, five units may be transferred to both Planning Area 2 and 4, giving each planning area a development cap of 105 units of residential development. In no cases, however, shall the total number of units on the site exceed 300 residential units.

Planning Area 6 reflects public uses, such as the Dominguez Channel, that are expected to remain on the site as they exist today. In the future, the City seeks to connect the project site with the utility easement (currently owned by Southern California Edison and located outside the Specific Plan) for use as an open space and recreation area. A system of pedestrian and/or vehicular bridges may be created at a later date. Additionally, the service roads at the rear of the project site will cross property owned by the Los Angeles County Flood Control District. Access easements will need to be secured for development to occur.



4.2.3 Conceptual Site Plan

A conceptual site plan has been provided as an example of the varied mix of commercial and residential uses that could be developed on the Artesia Corridor Specific Plan site. It is important to note that this layout is not the only configuration of buildings or mix of uses permitted, but instead is an example of a single development alternative that is achievable through the provisions of the Specific Plan. Future plans may vary so long as they are consistent with the Specific Plan.

The conceptual site plan provided in Figures 4.2c and 4.2d identifies possible locations of commercial, live/work, mixed-use and residential uses in relation to the “main street” concept that is required by the Specific Plan. Figure 4.2c identifies the amount, type, and location of development while Figure 4.2d provides a conceptual illustrative. Consistent with the market research performed, the conceptual site plan demonstrates the capacity to achieve a total of approximately 350,000 square feet of commercial uses: approximately five mid-size retail uses (each approximately 25–30,000 square feet in size), 30,000 square feet of restaurant uses, and 28,000 square feet of support commercial businesses (typically smaller retail uses such as florists, coffee shops, or gift shops).

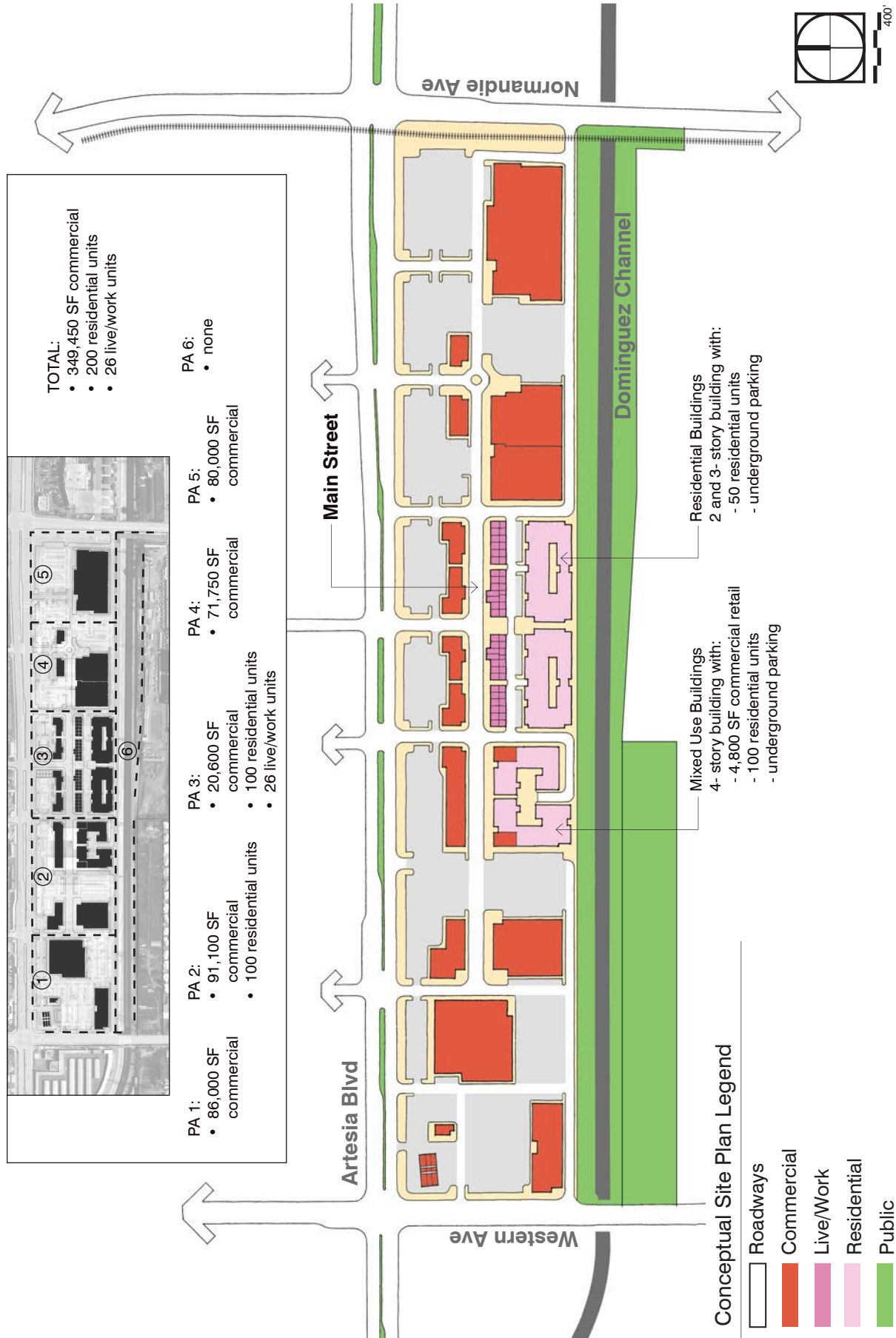
In the conceptual site plan, mid-size commercial tenants are positioned as retail “anchors” to the site, and restaurant and support commercial uses are positioned along the north side of the internal “main street” to enhance the walkability of the central core while also maintaining visibility along Artesia Boulevard. The conceptual site plan proposes to mirror the support retail uses on the south side of the “main street,” along with an urban residential village component of 26 live/work units with 10,000 square feet of office uses, and 200 residential units ranging from approximately 1,000–1,800 square feet per unit. These dwelling units are carefully positioned away from the hustle and bustle of Artesia Boulevard to the more peaceful open space edge of the Dominguez Channel. The combination and intensity of these mixed uses, integrated with strategically located and aesthetically pleasing plazas, outdoor dining spaces, and sitting areas create social gathering opportunities and ultimately generate a desirable walking, shopping, working, and living experience. Subterranean parking or a parking garage would be necessary to accommodate commercial square footage above and beyond 350,000 square feet on this project site.

4.3 CONCEPTUAL CIRCULATION PLAN

The ability to easily access and travel within the project site by both car and foot is of paramount importance to the project’s success. The Circulation Plan (Figure 4.3a) establishes an internal main street that promotes a pedestrian friendly atmosphere while also enhancing vehicular circulation along the perimeter roadways and within the site.

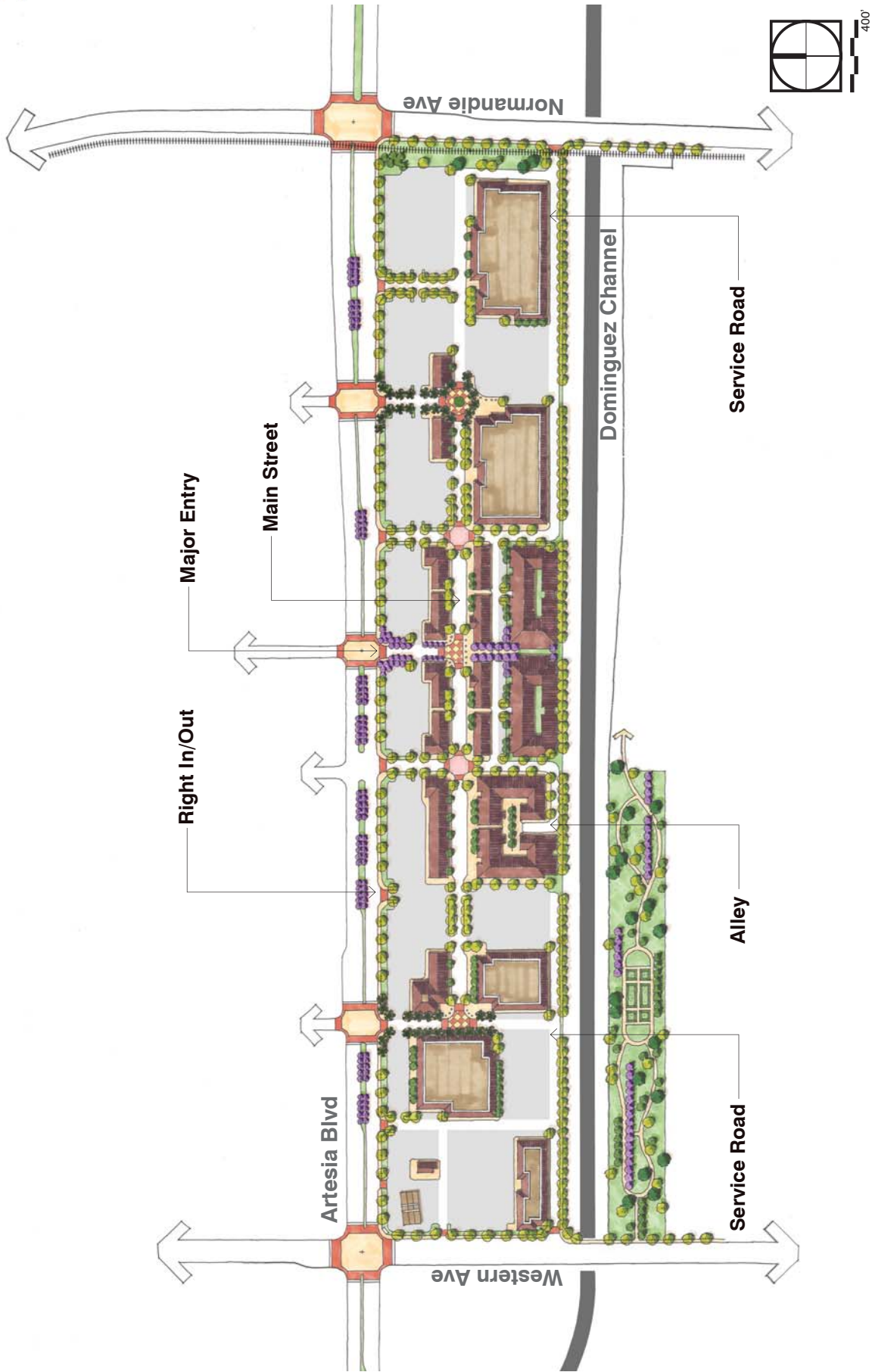
Access points will be strategically placed along Artesia Boulevard, Western Avenue, and Normandie Avenue to allow traffic to easily access the parking areas and the main street. The main street serves as the site’s circulatory backbone and ensures safe and efficient access to all portions of the site for pedestrians and automobiles. The main street also distinguishes the site as a unique place to live and shop in Gardena, offering an alternative to the typical power center. Service roads are also provided at the rear of the site to allow service vehicles to access the majority of commercial uses without disturbing the traffic flow and pedestrian activity along the main street and Artesia Boulevard.

All roadways within the project area will be constructed according to the minimum standards and guidelines set forth in the Specific Plan. All roadways within the project area are private and will be maintained through a property owner’s association. The lights along and surrounding Artesia should be synchronized to improve traffic flow.



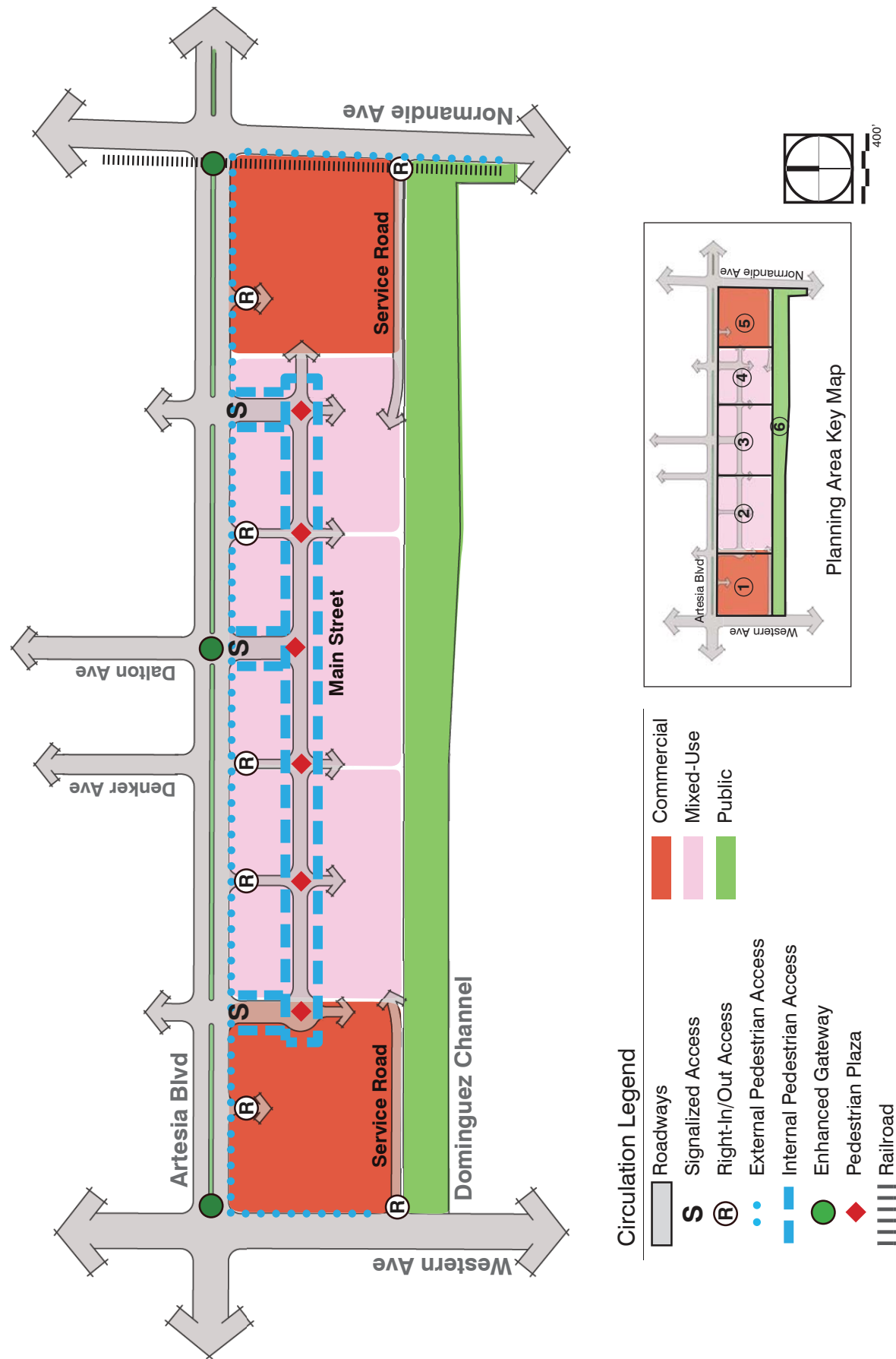
note: The plan illustrated is conceptual in nature. Future plans may vary so long as they are consistent with the Specific Plan. source: The Planning Center, 2006

Figure 4.2c Conceptual Site Plan



note: The plan illustrated is conceptual in nature. Future plans may vary so long as they are consistent with the Specific Plan. source: The Planning Center, 2006

Figure 4.2d Conceptual Illustrative Plan



source: The Planning Center, 2006

Figure 4.3a Conceptual Circulation Plan





The typical roadway standards for the Artesia Corridor Specific Plan are based on the unique character of each street. The following are brief descriptions of each street classification, with illustrations of each street section provided on the following pages, beginning with Figure 4.3b.

4.3.1 Perimeter Access and Roadways

Access Points

The Specific Plan provides for at least ten consolidated points of access to provide safe and efficient entry to and exit from the site, and to enhance traffic flow and safety along Artesia Boulevard. Three signalized access points are located along Artesia Boulevard and aligned to access points directly opposite, along the northern side of Artesia Boulevard. These signalized access points are identified as Main Entries and serve as the primary access routes from Artesia Boulevard to the primary internal street, identified as the Main Street. In addition, there are seven secondary access points off of the surrounding major roads to transfer traffic into the project. These access points are not signalized, and are classified as Right In/Out Entries. All of the access points are strategically placed along the three edges of the site bordering major streets.

The three locations for the Main Entries are evenly distributed along Artesia Boulevard. The first is located at the eastern edge of Planning Area 1, the second opposite Dalton Avenue at the center of the site, and the third is placed just west of Planning Area 5. There are two different street sections for Main Entries: Main Entry with Pathways and Main Entry without Pathways. Both of these street sections will carry the bulk of traffic entering the project and serve as gateways to the site. Accordingly, the landscape and streetscape along the Main Entries should be distinctive and more prominent in character than other access points. The flexibility to choose between these two street sections for Main Entries accommodates the need to adapt to future site plan conditions.

The Main Entry with Pathways functions as a major gateway for pedestrians in addition to providing vehicular access. The dimensions depicted in Figure 4.3c allow for adequate parkways to accommodate the comfortable passage of pedestrians from Artesia Boulevard to the main street. Landscaping should augment the buffer space between the walkway and travel lanes, as well as between the walkway and building face.

Although the Main Entry without Pathways also serves as a primary access route from Artesia Boulevard to the Main Street, this street section does not provide for pedestrian access to the project. Figure 4.3d illustrates the landscaped parkway on either side of the paved travel lanes. Because this street section does not provide for sidewalks within the parkway, a major pedestrian passageway to the Main Street must be provided within 75 feet of the Main Entry without Pathways street in order to provide pedestrian access to the site.

Seven Right In/Out Entries are also provided, with five along Artesia Boulevard and two service access points at the rear of the site along Western and Normandie Avenues. The two service roads would not be permitted to connect at the center of the site to prevent their use as an alternate route for Artesia Boulevard.

The purpose of the Right In/Out Entry is to provide secondary access routes from Artesia Boulevard to the Main Street. The five access points distributed along Artesia Boulevard are generally evenly placed in conjunction with Main Entries. The Right In/Out Entry will be used primarily for the movement of vehicles to and from residential and commercial uses on the site, and is not intended for pedestrian activity, as illustrated in Figure 4.3e.



Artesia Boulevard

The Artesia Boulevard right-of-way along the northern edge of the site will be improved to create a highly landscaped pedestrian walkway and enhance the visual experience of cars and people traveling along Artesia Boulevard. The building setback is 22 to 24 feet, as shown in Figure 4.3f, to allow a pedestrian to comfortably walk along a meandering walkway, shielded from traffic by landscaping.

Western Avenue

Similar to Artesia Boulevard, the intent of the setback is to buffer the pedestrian and building use from traffic along this corridor. The setback is 16 feet, as shown in Figure 4.3g, to provide a pedestrian friendly atmosphere while recognizing the smaller nature of Western Avenue.

Normandie Avenue

The setback from Normandie Avenue is unique in that it accommodates the Union Pacific Railroad easement that lies between the western project edge and the existing street. The purpose of the setback from the railroad easement to the building face is to establish a sufficient buffer from air quality impacts and noise associated with the railroad, as illustrated in Figure 4.3h. A pedestrian walkway is allowed within this setback, although pedestrian activity is not emphasized along this edge of the project.

Dominguez Channel Setback

The setback from the Dominguez Channel easement along the southern edge of the site allows for a minimum space for utility and service access between the easement edge and building, as shown in Figure 4.3i. Where possible, this setback should be widened to allow for adequate space in which a pedestrian may comfortably walk in a well landscaped place between the easement edge and building face.

4.3.2 Internal Access and Roadways

Main Street

The key component of the Circulation Plan is the concept of a primary internal roadway, categorized as the Main Street and illustrated in Figure 4.3j, that runs in an east-west direction through the site, parallel to Artesia Boulevard. Elements along the Main Street should be designed to create an attractive, lively, and pedestrian-oriented core. Factors essential to this goal are the establishment of safe and secure environments, treatment and maintenance of pedestrian spaces, and building orientation to these spaces. There are many design options that can be used to make the Main Street a pedestrian friendly place and it will be critical for applicants to work closely with the City to create the best possible design for the site. The





Main Street also provides efficient vehicular access and parking for the different land uses. The design of the street should promote a unique street scene within the site and reinforce the space as the heart of the Artesia Corridor Specific Plan.

The Main Street functions as a throughway for both vehicles and pedestrians, and the emphasis on pedestrian spaces along the length of the street is important to the establishment of the Artesia Corridor as a landmark place. It is envisioned that the Main Street will have sidewalks wide enough to accommodate landscaping and pedestrian walkway areas as well as space for outdoor dining tables, window shopping, and architectural projections such as awnings. The width of the sidewalk area may also vary to allow for nodes of public spaces such as the central linear pedestrian plaza, and smaller courtyards and plazas. The fluctuating width of the parkway creates an interesting dynamic for the pedestrian in contrast to the traditional static, linear walkway.

The design of streetscape elements such as street furniture, landscaping, signage, and lighting is important in creating unified and visually pleasing pedestrian areas. Landscaping and street furniture should work together to create an attractive, lively, and pedestrian-oriented feel along the length of the Main Street. Clusters of canopy shade trees and outdoor seating should be used to provide areas protected from the sun. Ample outdoor eating and seating opportunities will provide for pedestrian convenience and create social opportunities. To create visual continuity throughout the site, the design of street furniture, landscaping, signage, and lighting shall be coordinated. As well, special paving features and public art add richness to the environment and should be incorporated into the design of public spaces when appropriate.

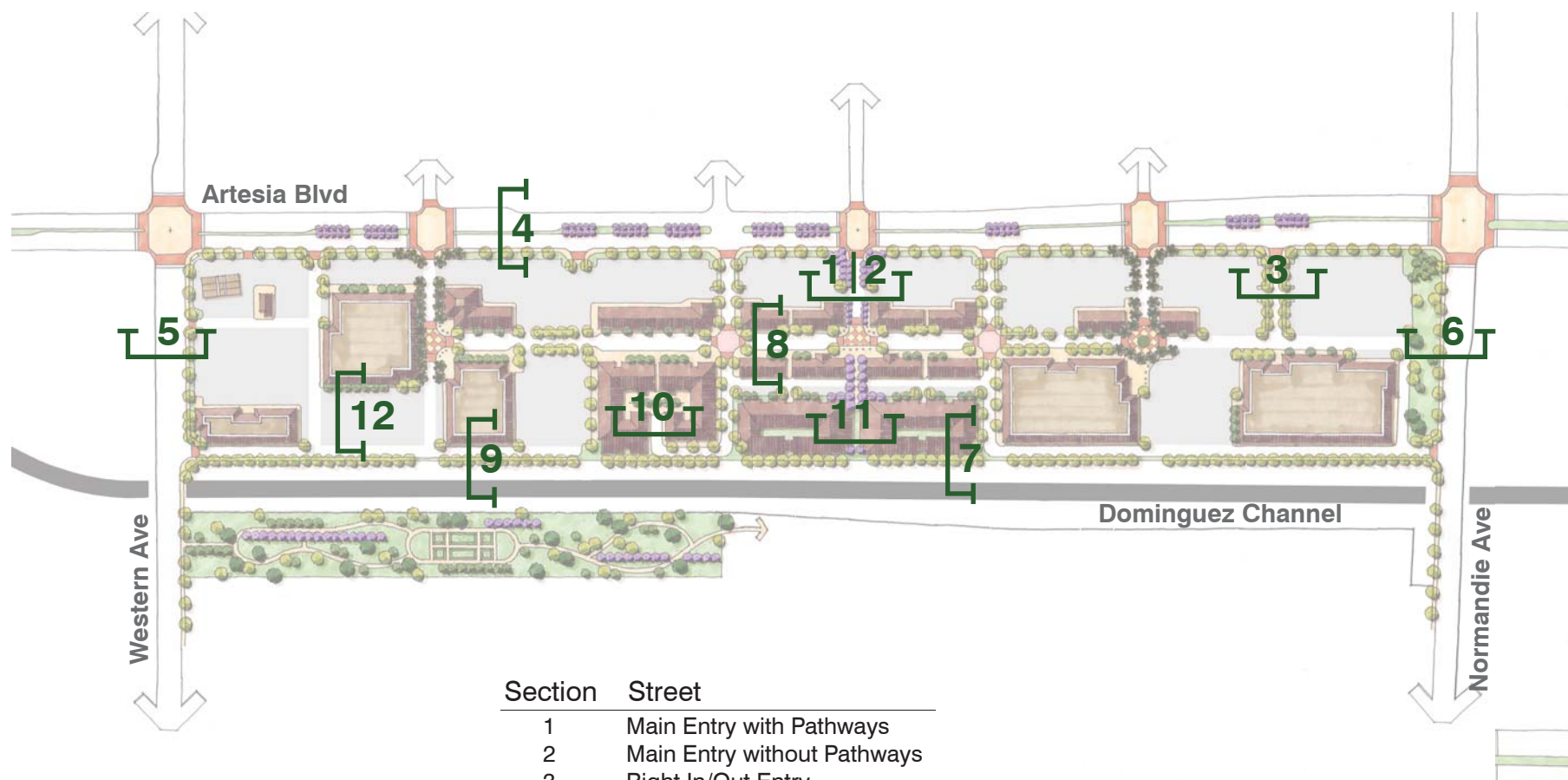
The design and orientation of structures surrounding the Main Street also affect the function and character of pedestrian spaces along the street. Buildings should front onto the Main Street to create a visually pleasing streetscape and enhance pedestrian activity. Attention should also been given to the modulation and articulation of building facades along the Main Street, framing the pedestrian areas with quality architecture.

As the spine for internal vehicular circulation, the Main Street also carries traffic throughout the project. To balance the dual functions as a pedestrian and vehicular spine and maximize pedestrian safety and comfort, neck downs, bulb-outs (which can also act as planters), and traffic calming measures are essential components to this concept.

Alternating parking bays on either side of the Main Street also serve to buffer pedestrians from moving vehicles, and allow the Main Street to provide parking to adjacent land uses. Parking along the Main Street may either be head-in diagonal or parallel and should act as a traffic calming mechanism that encourages drivers to be more cautious as they drive. In addition to parking along the main street, the bulk of parking will be provided in parking lots behind the buildings.

Service Road

The objective of the Service Road classification is to provide service vehicle access to land uses within the project site; pedestrian activity along this route is not a priority. The Service Road, shown in Figure 4.3k, has two access points planned to serve the project. These access points should occur off of either Western Avenue or Normandie Avenue, adjacent to the Dominguez Channel. These roads are not intended to carry large amounts of traffic, and shall not connect to form an alternate route parallel to Artesia Boulevard.



| Section | Street |
|---------|-----------------------------|
| 1 | Main Entry with Pathways |
| 2 | Main Entry without Pathways |
| 3 | Right In/Out Entry |
| 4 | Artesia Boulevard |
| 5 | Western Avenue Service Road |
| 6 | Normandie Avenue |
| 7 | Dominguez Channel Setback |
| 8 | Main Street |
| 9 | Service Road |
| 10 | Alley |
| 11 | Pathways between buildings |
| 12 | Parking areas |

Figure 4.3b Key Map for Street Sections





40' maximum building height

a) 4' landscaped parkway

b) 4' sidewalk

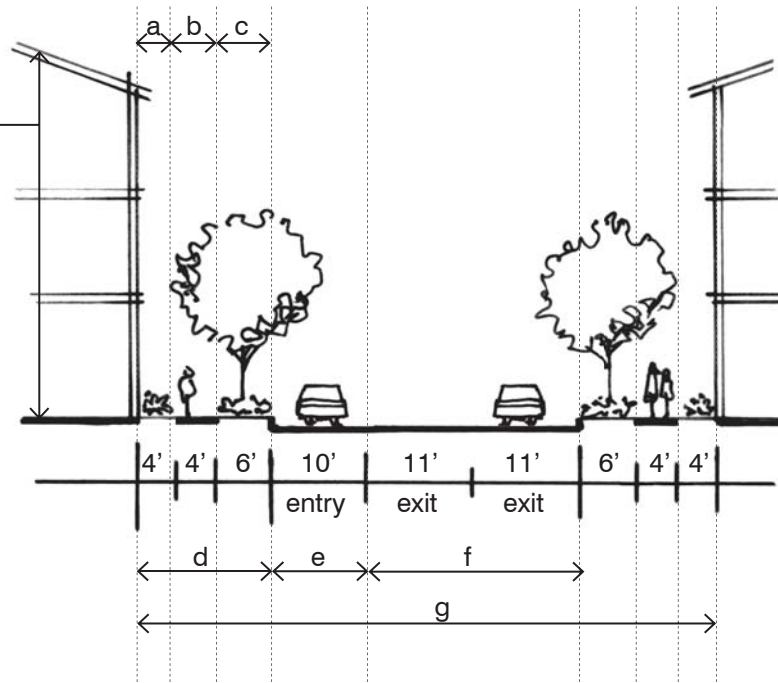
c) 6' landscaped parkway

d) 14' from curb to building

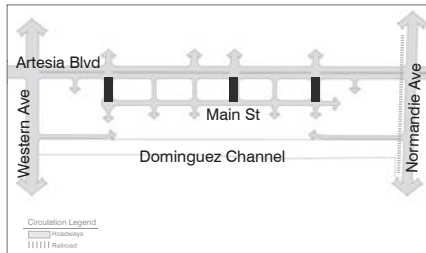
e) 10' for single travel lane, shown here as an entry lane

f) 11' per travel lane (when 2 or more lanes), shown here as 2 exit lanes

g) 60' total right-of-way



Key Map



Description

To encourage creativity and flexibility in future site plans, there are two configurations allowed for the project's main entry points. The Main Entry with Pathways roadway classification is intended to provide the primary access routes from Artesia Boulevard to the Main Street for both vehicles and pedestrians. The right-of-way for the Main Entry with Pathways may vary according to the number of entry and exit lanes required per traffic studies. A 10' wide travel lane is required for single lanes in any one direction, and an 11' wide travel lane is required for two or more travel lanes in any one direction.

Because the Main Entry with Pathways serves as a major gateway to the project, prominent landscaping should be used to enhance these throughways. The 4' pedestrian walkway within the 14' parkways on either side of the road should be cushioned from traffic by adequate landscaping. The maximum building height along the Main Entry is 40', with architectural projections allowed up to an additional 10'.

Figure 4.3c **Section 1: Main Entry with Pathways**



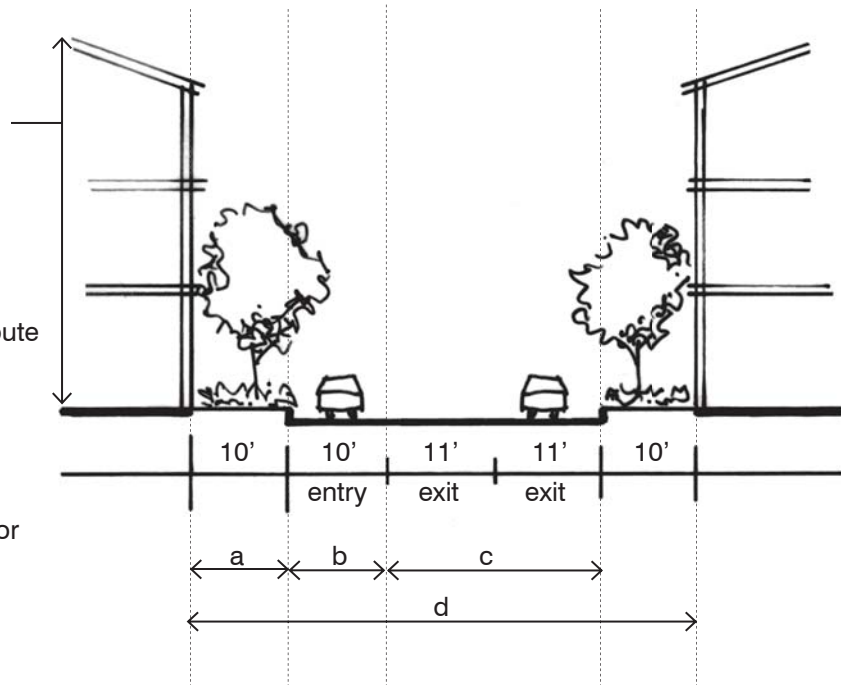
40' maximum building height

a) 10' landscaped parkway from building to curb; with no walkways incorporated within the setback provided that an alternate pedestrian access route is constructed within 100' of major entry

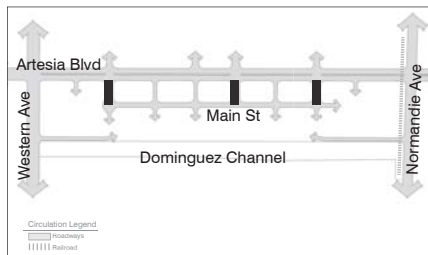
b) 10' for single travel lane, shown here as an entry lane

c) 11' per travel lane (when 2 or more lanes), shown here as 2 exit lanes

d) 52' total right-of-way



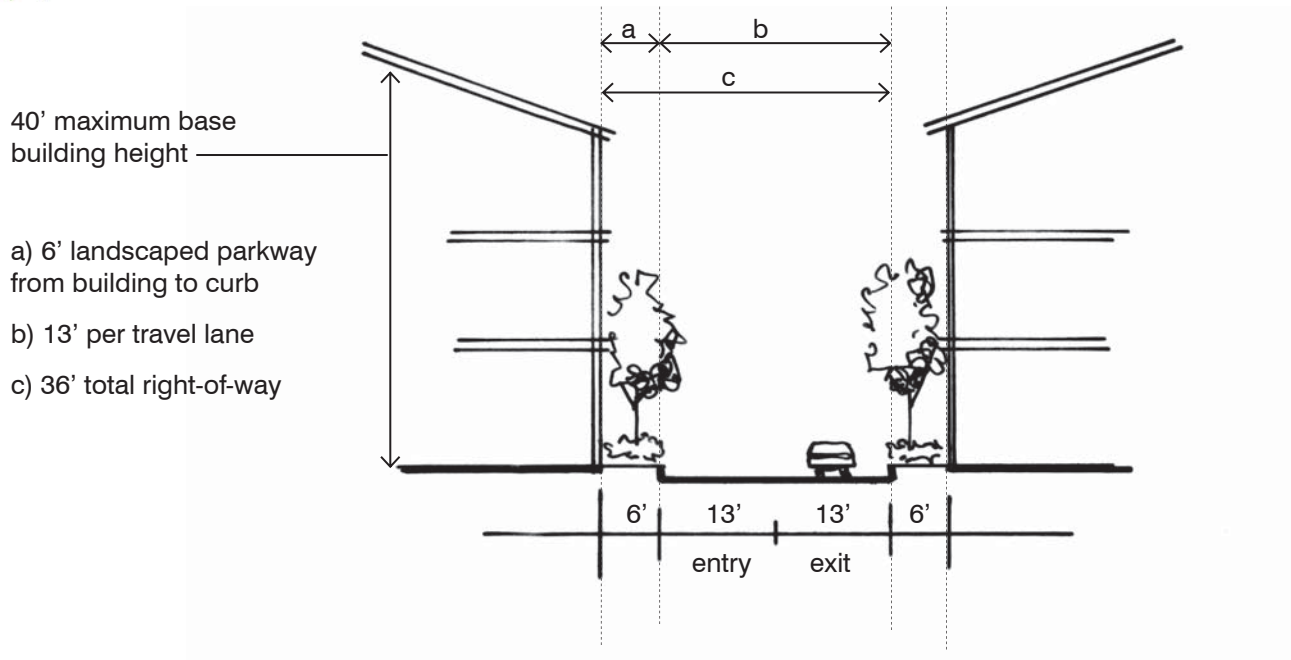
Key Map



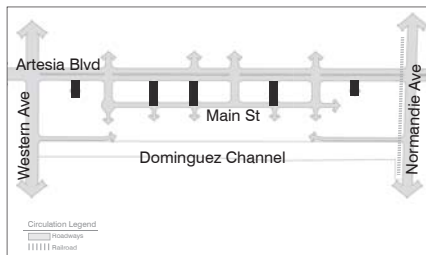
Description

To encourage creativity and flexibility in future site plans, there are two configurations allowed for the project's main entry points. The Main Entry without Pathways classification shares a similar intent as the Main Entry with Pathways without providing pedestrian access. The number and dimension of travel lanes are the same as those depicted for the Main Entry with Pathways. The parkways for the Main Entry without Pathways is reduced to a 10' landscaped building setback without a pedestrian walkway. Where this roadway is applied, a major pedestrian throughway must be provided within 75 feet of the Major Entry without Pathways to provide pedestrian access from Artesia Boulevard to the Main Street.

Figure 4.3d Section 2: Main Entry without Pathways



Key Map



Description

The Right In/Out Entry provides secondary access routes from Artesia Boulevard to the Main Street. Five access points have been planned for the Right In/Out Entry, evenly distributed along Artesia Boulevard in coordination with Major Entries. The design of the street incorporates two 13' travel lanes with adjacent 6' parkways providing buffer space between the road and building. The maximum height for buildings along the Right In/Out Entry is 40', with an additional 5' allowed for architectural projections. Pedestrian access is not required for the Right In/Out Entry.

Note: Roadway widths shown are subject to Los Angeles County Fire Regulations and may be increased or decreased as determined by the building height, use, and relationship to surrounding buildings and roadways.

Figure 4.3e Section 3: Right-In/Out Entry

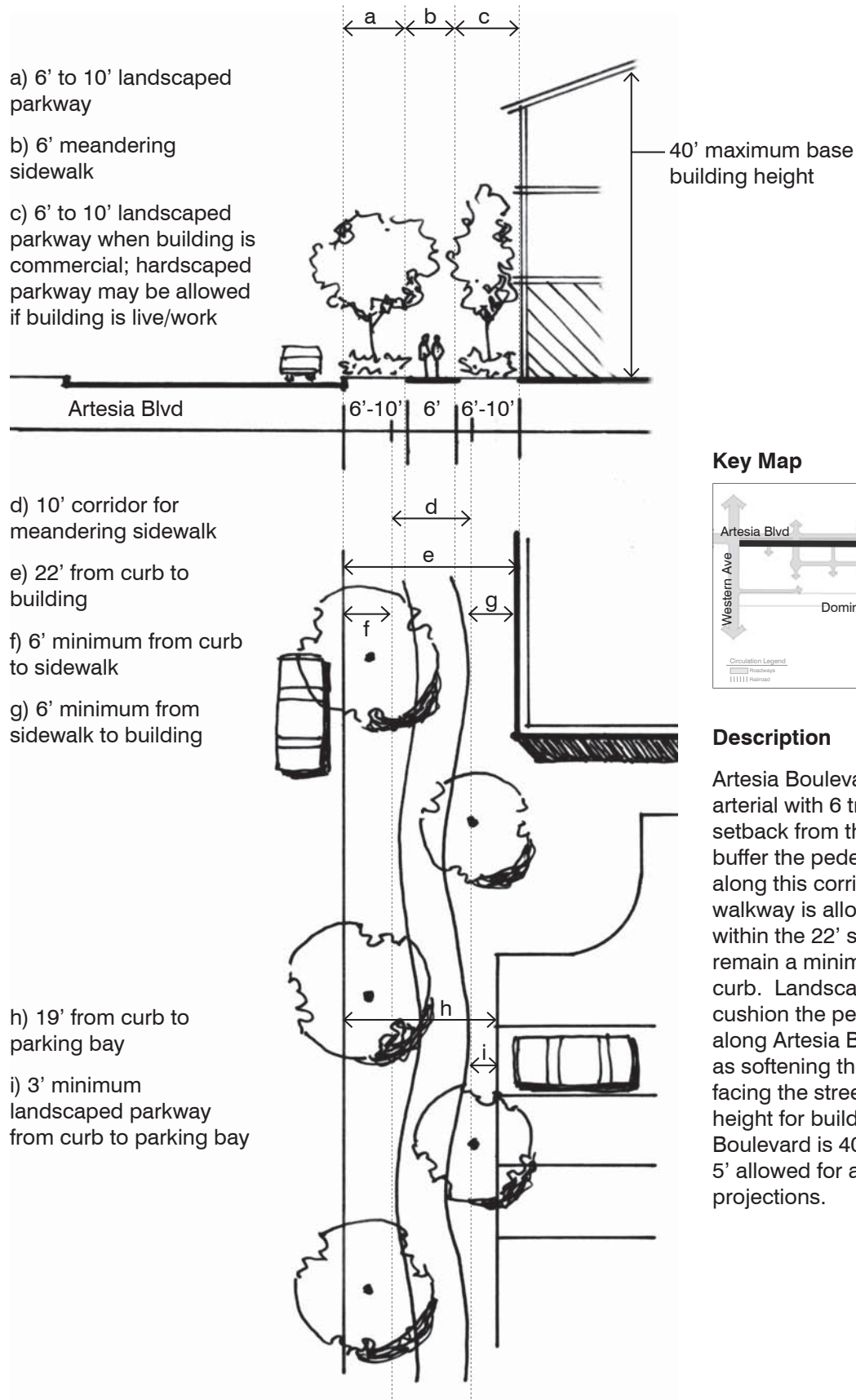
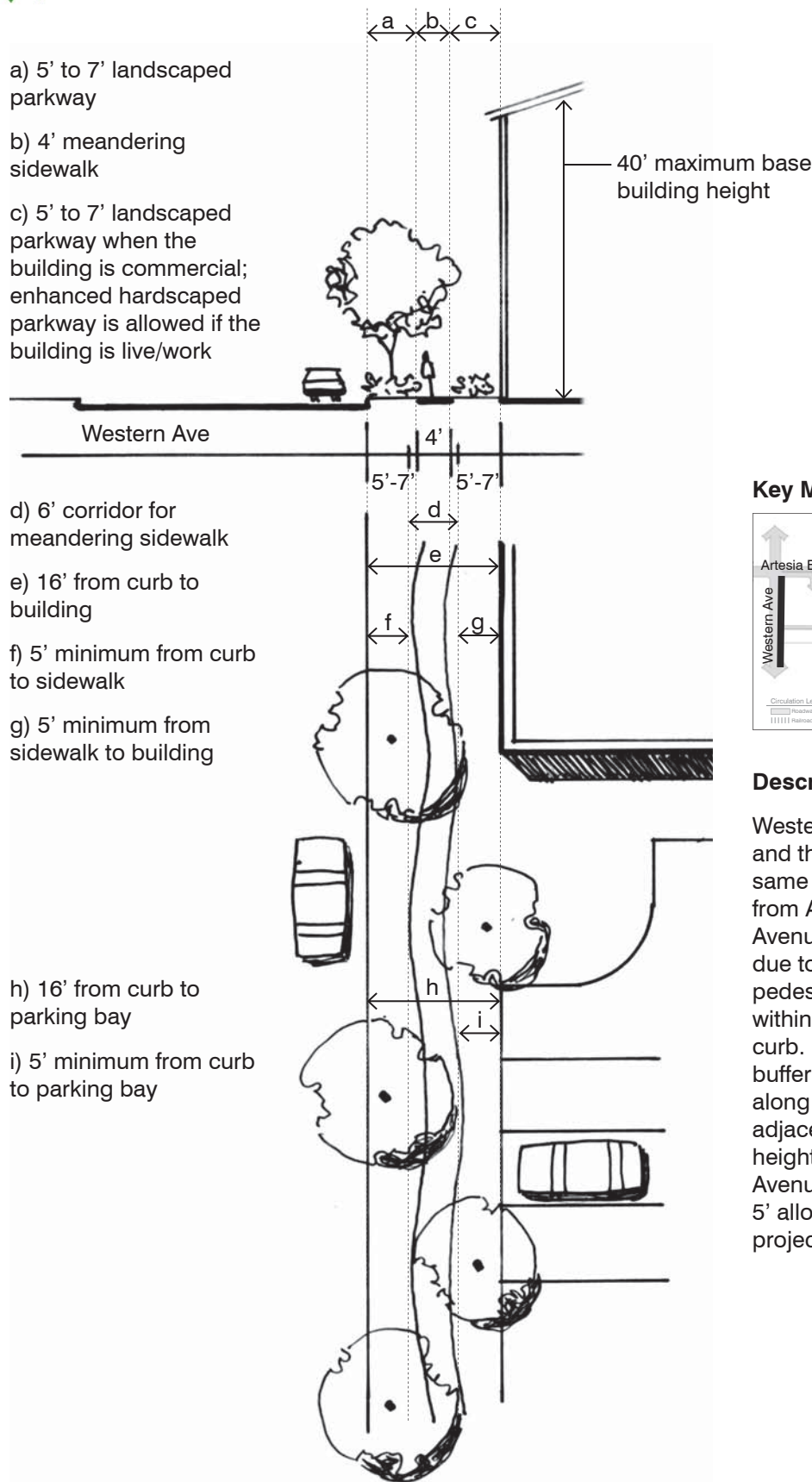
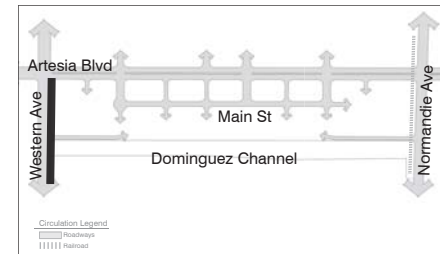


Figure 4.3f Section 4: Artesia Boulevard



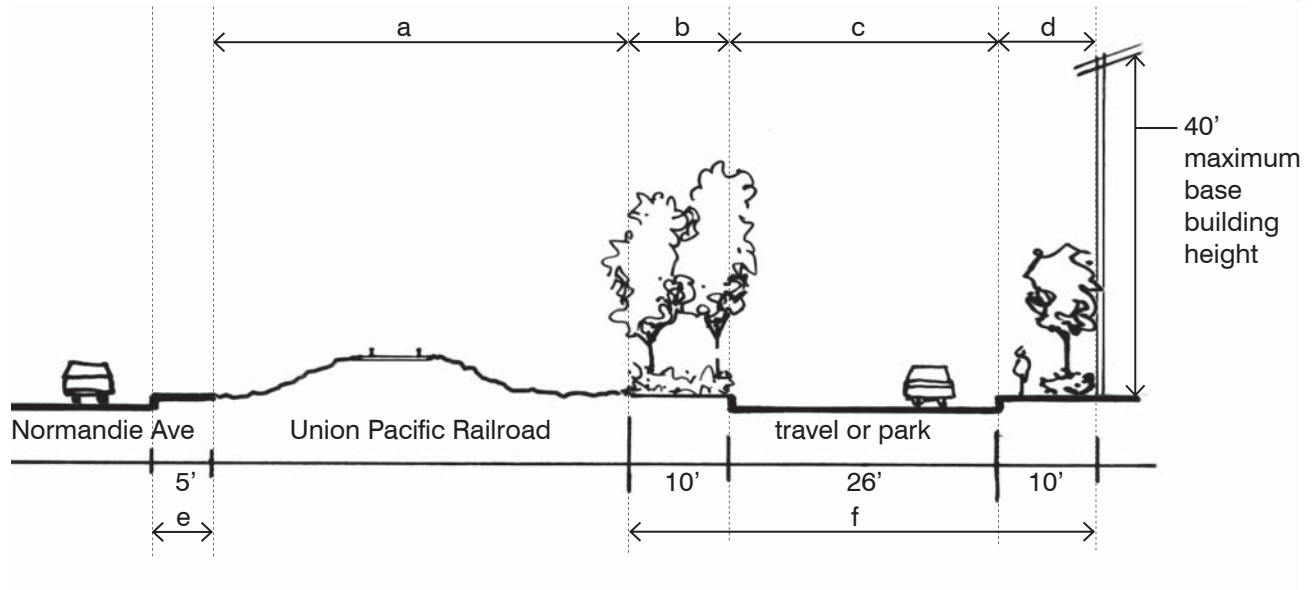
Key Map



Description

Western Avenue is a minor arterial, and the 16' setback shares the same objective as the setback from Artesia Boulevard. Western Avenue has four travel lanes, and due to the lesser classification, the pedestrian walkway may come within a minimum of 5' from the curb. Landscaping should aim to buffer the pedestrian from traffic along the street, as well as from the adjacent building. The maximum height for buildings along Western Avenue is 40', with an additional 5' allowed for architectural projections.

Figure 4.3g Section 5: Western Avenue



a) 75' railroad easement

b) 10' landscaped setback

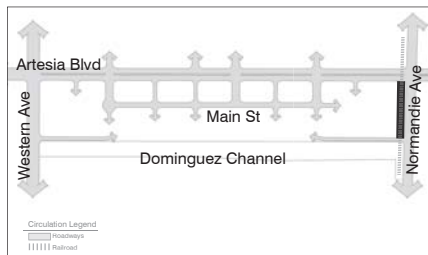
c) 26' service road

d) 10' building setback

e) 5' minimum sidewalk

f) 46' setback from railroad easement to building edge

Key Map



Description

The minimum setback dimension from the Union Pacific Railroad easement allows for a 10' landscaped easement, 26' minimum service road, and 5' walkway within a 10' building setback. This setback intends to provide sufficient buffer space from noise associated with the railroad, and pedestrian activity within this setback should be accommodated but not emphasized. The service road may be expanded to allow for parking bays to serve the adjacent commercial use. The maximum height for buildings along Normandie Avenue is 40', with an additional 5' allowed for architectural projections.

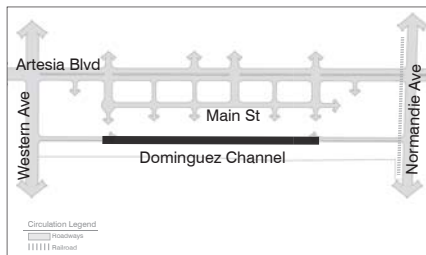
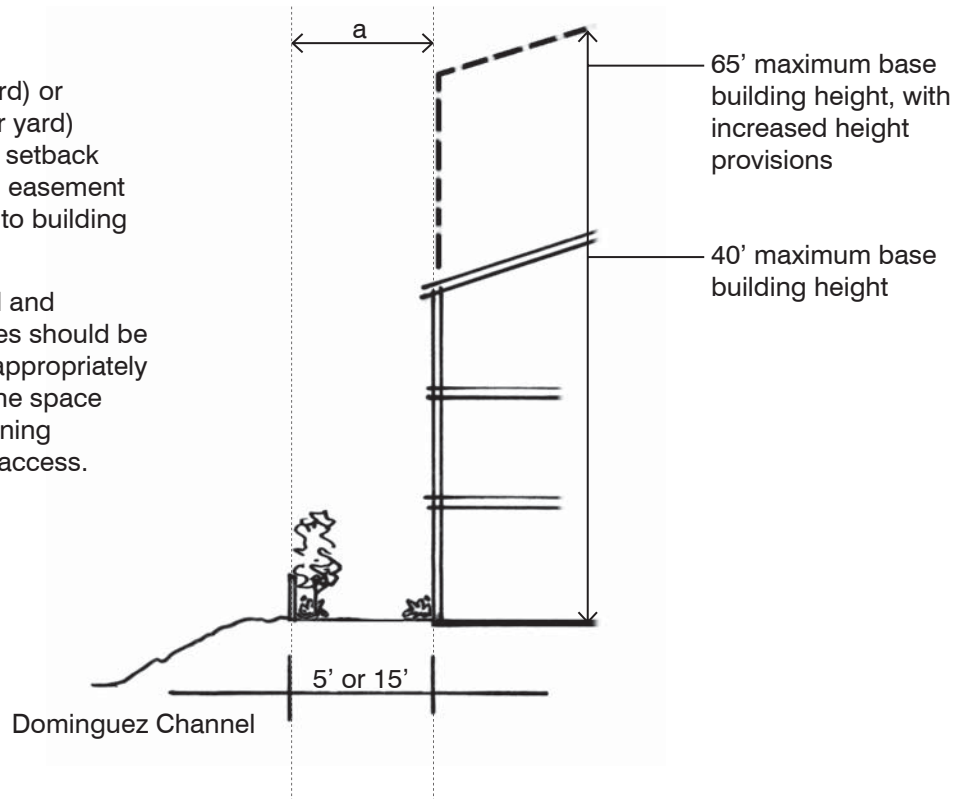
Note: Roadway widths shown are subject to Los Angeles County Fire Regulations and may be increased or decreased as determined by the building height, use, and relationship to surrounding buildings and roadways.

Figure 4.3h **Section 6: Normandie Avenue**



a) 5' (side yard) or 15' (front/rear yard) maintenance setback from channel easement property line to building edge

Both channel and building edges should be landscaped appropriately to enhance the space while maintaining security and access.

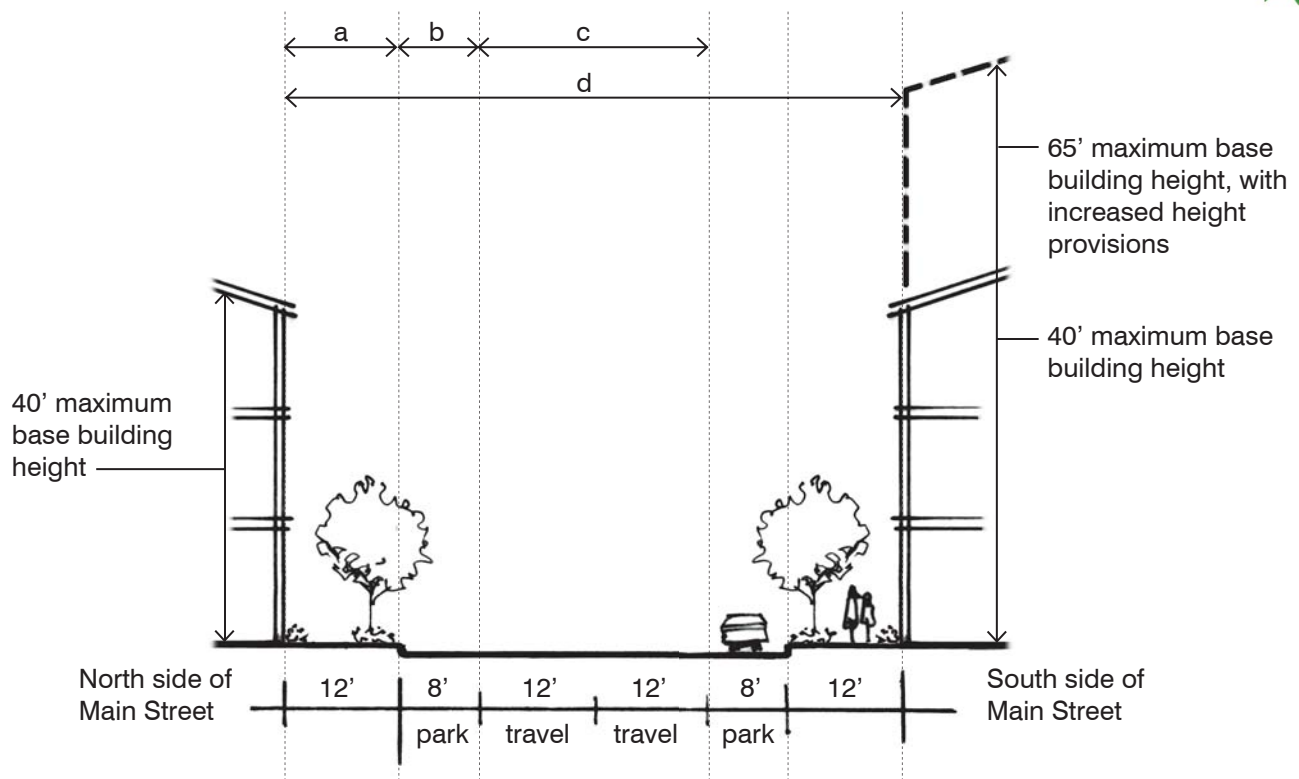


Description

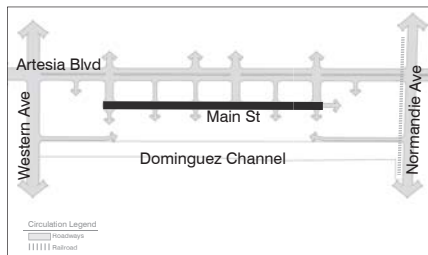
The desired setback from the Dominguez Channel easement is 15' to allow for a pedestrian passageway in addition to landscaping between the easement wall and building face. If a 15' setback is not possible, a minimum setback of 5' is required to allow for minor service and utility access behind the building. The maximum height for buildings along the Dominguez Channel varies from 40' to 65' depending on the proposed land use. Architectural projections are allowed up to 5' for commercial use and 10' for mixed use areas.

Note: Roadway widths shown are subject to Los Angeles County Fire Regulations and may be increased or decreased as determined by the building height, use, and relationship to surrounding buildings and roadways.

Figure 4.3i **Section 7: Dominguez Channel Setback**



Key Map



- a) 12' enhanced pedestrian pathway and building setback
- b) 8' parallel parking; 15' for diagonal parking; 20' for head-in parking
- c) 12' per travel lane
- d) 64' total right-of-way

Description

The Main Street will be the signature element of the Circulation Plan. The street aims to connect the internal land uses, provide access and parking to adjacent land uses, and create a safe and unique pedestrian environment. For vehicular circulation, the Main Street provides two travel lanes, each 12' in width, in addition to parking bays. The travel lanes are required to be 12 feet to provide additional space and safety for persons using the parallel parking spaces.

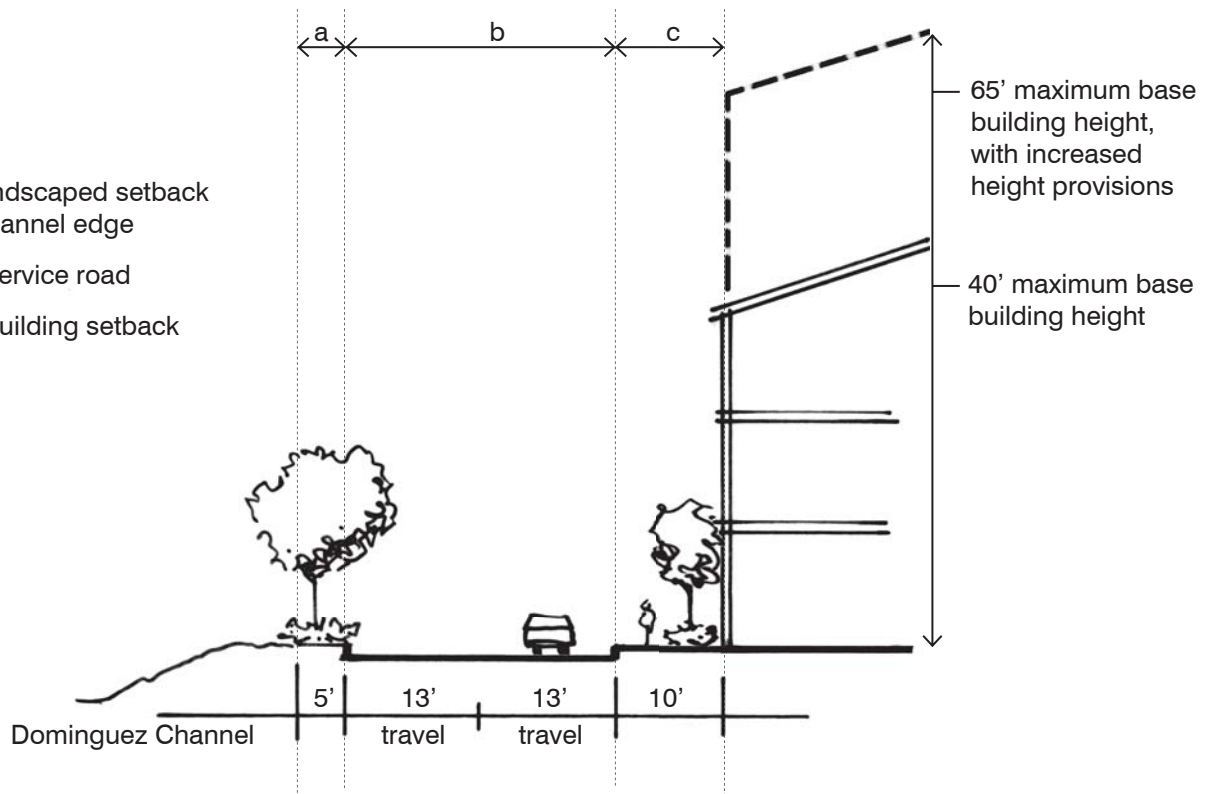
The use of the parking bays are flexible. Parking spaces along the Main Street are intended to provide convenient customer and guest parking. The parking may be parallel, diagonal, or head-in, and may be eliminated at points along the street, or expanded to provide parallel or diagonal parking on either side of the street. Traffic calming measures should be taken along the length of the street to ensure safety.

In addition to vehicular circulation, the Main Street intends to provide an array of public spaces for the pedestrian. Parkways shall be a minimum of 12', but are encouraged to widen and create variation along the length of the street. Further description relating to the intent and design of these pedestrian spaces may be found under Section 4.3.2, Internal Access and Roadways.

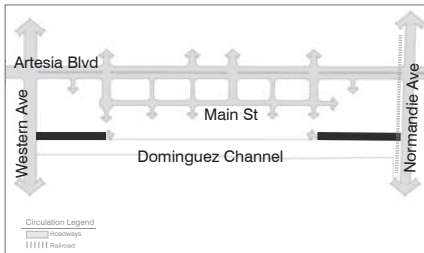
Figure 4.3j **Section 8: Main Street**



- a) 5' landscaped setback from channel edge
- b) 26' service road
- c) 10' building setback



Key Map



Description

The Service Road carries minor local traffic within the site and is specifically designed for service vehicles to access land uses within the project. Service Roads are planned to take access off of Western Avenue and Normandie Avenue, adjacent to the Dominguez Channel. Two 13' travel lanes are provided, with a 5' landscaped setback between the Dominguez Channel easement and curb. Pedestrian access is allowed along one side of the street, but pedestrian activity is not the focus of the streetscape. Building height varies from 40' to 65' depending on the adjacent land use. Architectural projections are allowed up to 5' for commercial use and 10' for mixed use areas.

Note: Roadway widths shown are subject to Los Angeles County Fire Regulations and may be increased or decreased as determined by the building height, use, and relationship to surrounding buildings and roadways.

Figure 4.3k Section 9: Service Road



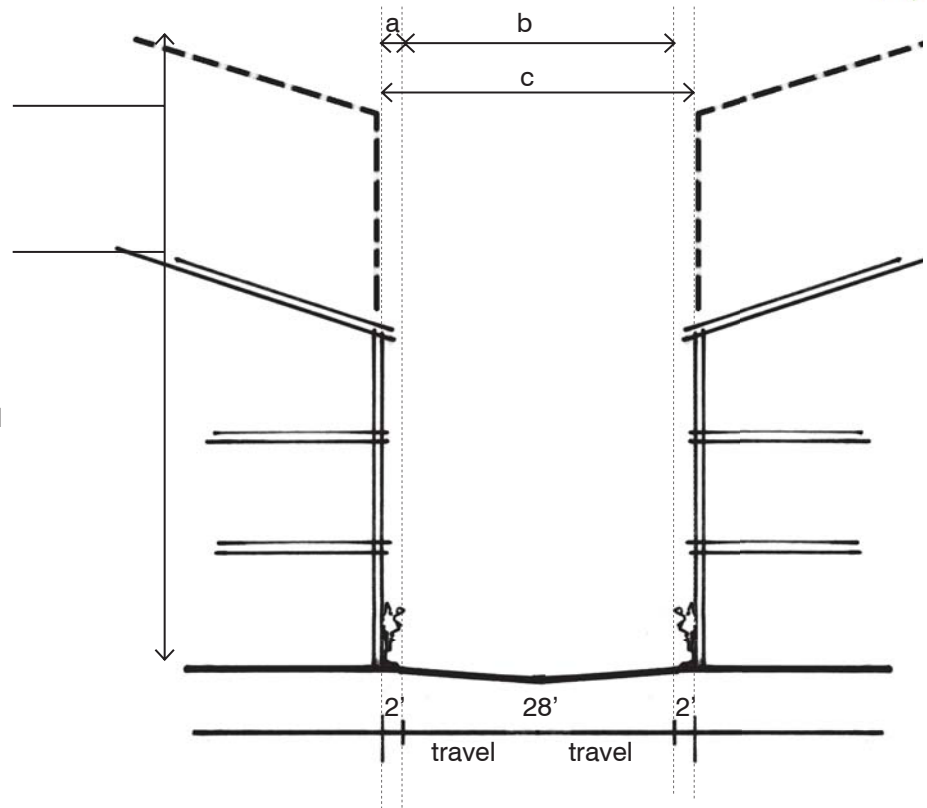
65' maximum base building height, with increased height provisions

40' maximum base building height

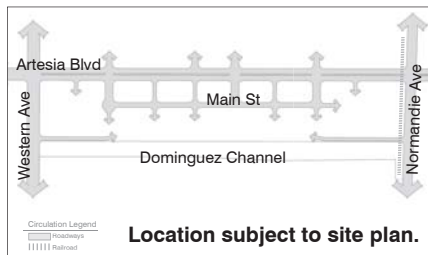
a) 2' minimum in-ground landscaping apron, 4' preferred

b) 28' roadway

c) 32' total right-of-way



Key Map



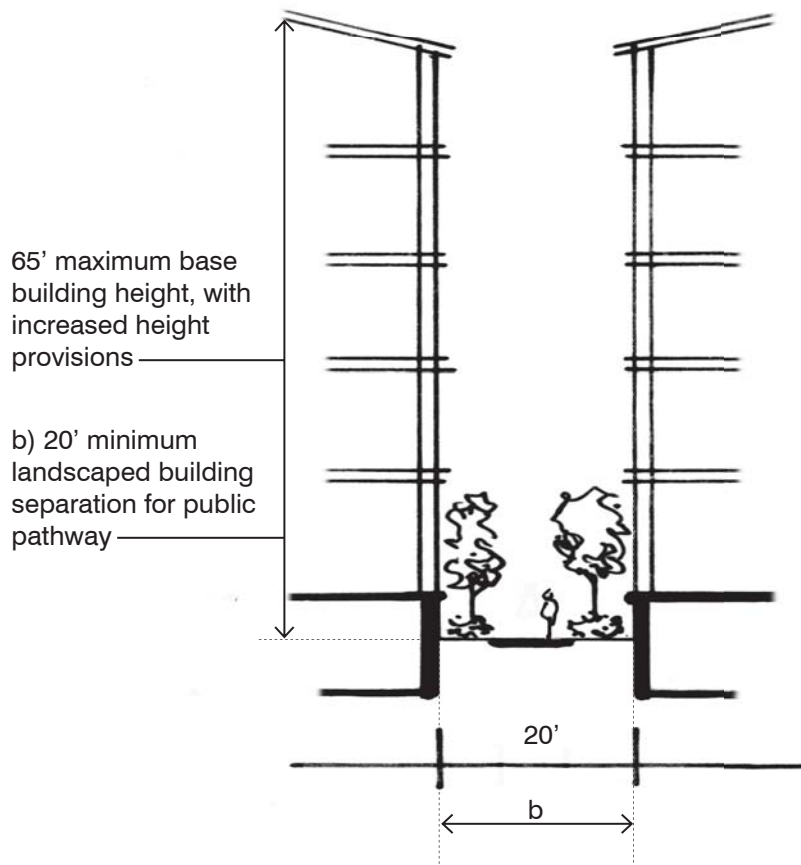
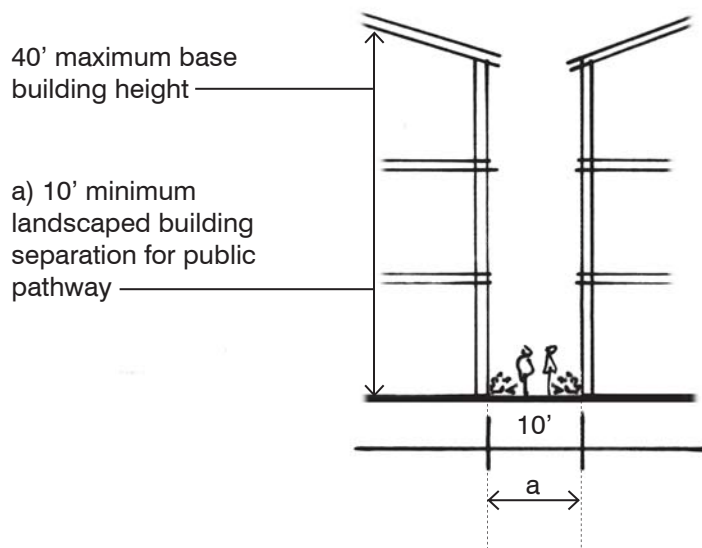
Description

The objective of the Alley classification is to provide access to areas within the project site, typically between buildings to allow access to parking areas. The 28' travel area accommodates two passing cars, and the 2' apron allows for landscaping between garages to soften the adjacent building facade. The landscaping apron of 2 feet may be reduced to six inches for alleys between live/work or residential uses that front the Main Street to maximize the width of the Main Street pathway.

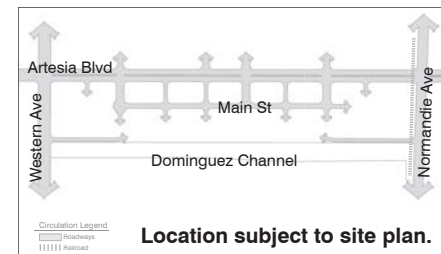
Notes:

1. Roadway widths shown are subject to Los Angeles County Fire Regulations and may be increased or decreased as determined by the building height, use, and relationship to surrounding buildings and roadways.

Figure 4.31 Section 10: Alley



Key Map



Description

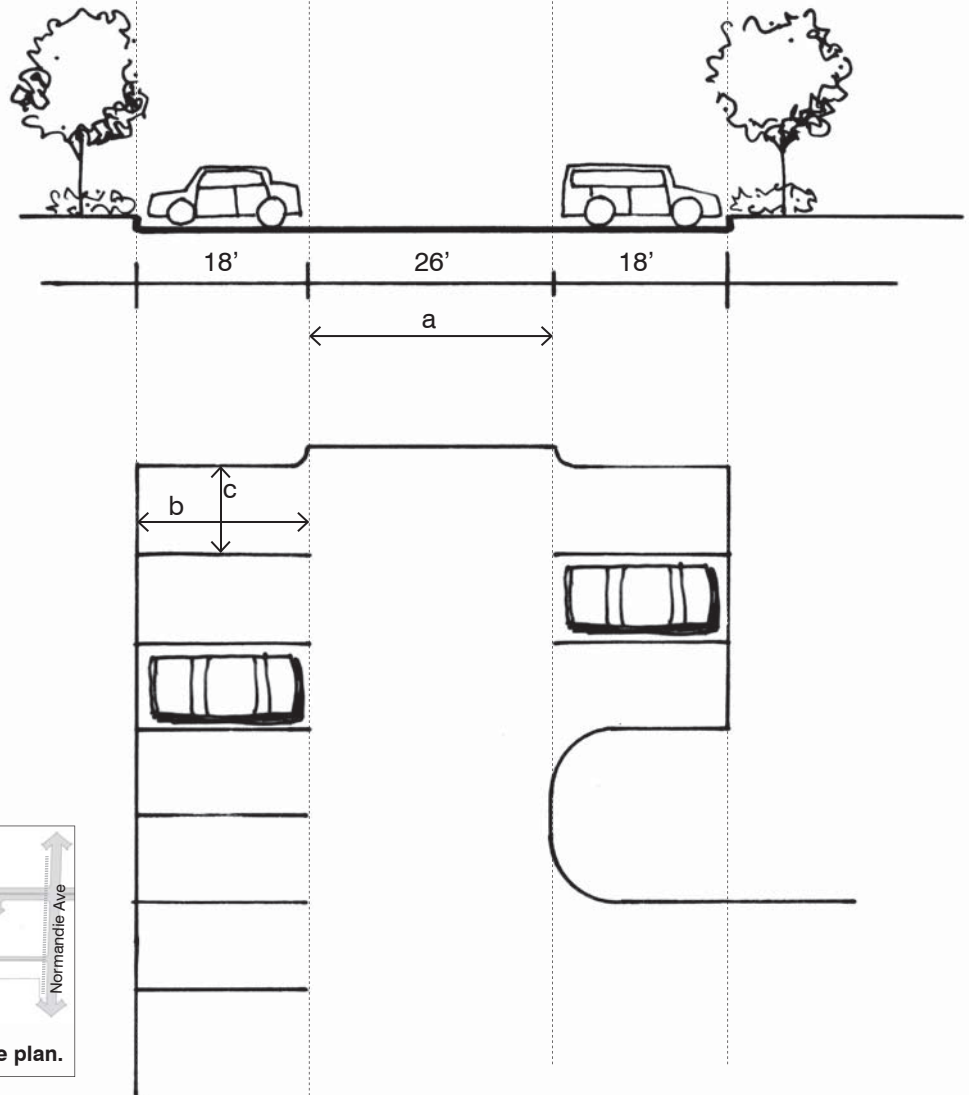
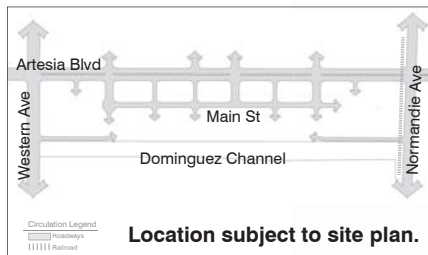
The minimum dimensions of pathways between buildings provide safety and comfort for the pedestrian. The 10' pathway is required for buildings with a maximum height of 40', and the 20' pathway is required for buildings with a maximum height of 65'. The pathway setback accommodates landscaping which should buffer the pedestrian and soften the adjacent building facade.

Figure 4.3m Section 11: Pathways Between Buildings



- a) 26' parking aisle
- b) 18' parking space length
- c) 9' parking space width

Key Map



Description

The typical minimum dimensions for parking spaces and parking aisles are shown above. The dimensions allow for the safe movement of vehicles in and out of parking spaces and through parking areas. Additional standards can be found in Section 5.3, Parking Standards.

Figure 4.3n **Section 12: Parking Dimensions and Aisles**

**Alley**

The objective of the Alley classification is to provide access to areas within the project site, typically between buildings to allow access to parking areas. Typical dimensions for an Alley are shown in Figure 4.3l.

Pathways between Buildings

The safety and comfort of pedestrians are major factors in determining the minimum dimension for pathways between buildings. The dimension

of pathways vary according to the height of the adjacent buildings, as illustrated in Figure 4.3m.

4.3.3 Pedestrian and Mass Transit

Pedestrian movement and mass transit are major elements in the Artesia Corridor Circulation Plan. The Main Street functions as the center of pedestrian activity within the project, while transit lines run along the three major streets bordering the project, with existing stops along Artesia Boulevard. The Main Entry roadways transfer pedestrians from the Main Street to the transit stops along Artesia Boulevard, and also function to move pedestrians from Artesia Boulevard to the Main Street. The pedestrian access ways are delineated in Figure 4.3a, and the transit routes and stops are illustrated in Figure 4.3o.

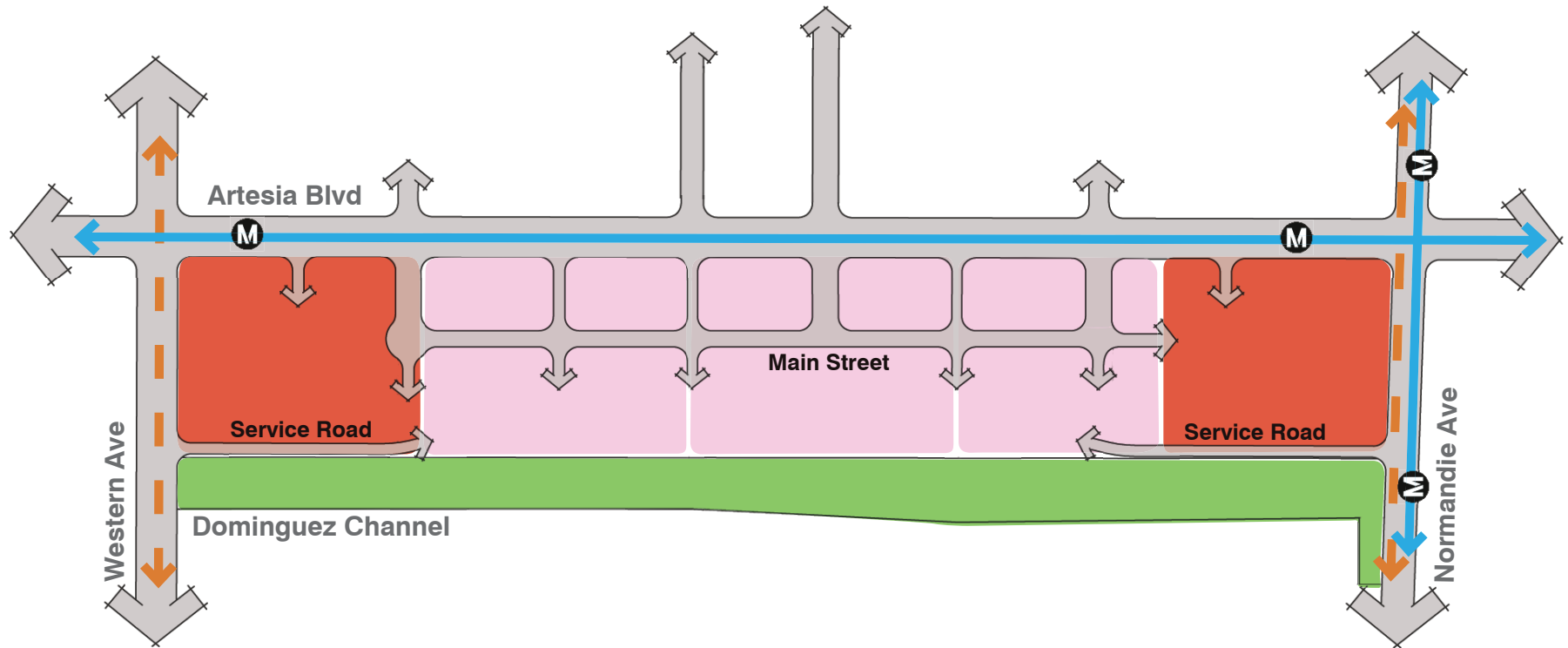
The internal pedestrian access ways are concentrated along the Main Street and Major Entry roadways within the Circulation Plan. The Main Street intends to emphasize pedestrian movement and public space, establishing an active human element centrally located within the project. Pedestrian movement along Artesia Boulevard will access the Main Street via the Major Entry with Pathways. Where there is a Major Entry without Pathways, pedestrian access to the Main Street will be provided within 75 feet of a Major Entry.

With transit lines running along the three major streets that border the Artesia Corridor Specific Plan, the site benefits from the ability to use mass transit and has the opportunity to attract additional transit service. Currently, Artesia Boulevard is served by the Los Angeles County Metropolitan Transportation Authority (LAMTA) Metro Lines 130, 210, 310, and 144. The LAMTA Metro Line 209 and the Gardena Municipal Bus Lines GA-1 and GA-2 operate along Normandie Avenue. The Gardena Municipal Bus Lines GA-1 and GA-2 also run along Western Avenue. In addition to the bus routes, the LAMTA Blue Line rail service operates in the vicinity of the project.




There is one existing transit stop along Artesia Boulevard, on the side of the street directly adjacent to the project. Due to the possible commercial activity and residential dwelling units permitted by the Artesia Corridor Specific Plan, there is an opportunity for future transit service to occur near or at the site.

4.4 GRADING PLAN

The topography of the project site is generally level with areas that are gently to moderately sloping. Artificial fill soils cover most of the site and are thickest near the center of the site where the Dominguez Creek was infilled and at the eastern end of the site where a sump was excavated and filled.



Public Transportation Legend

-  Roadways
-  LAMTA Bus Lines
-  Gardena Municipal Bus Lines

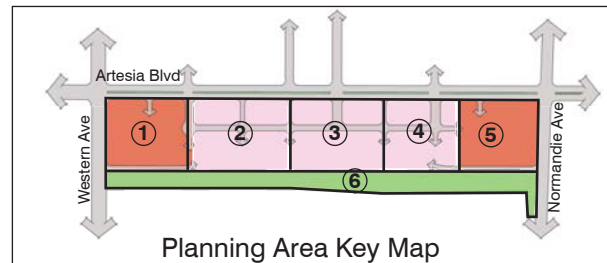


Figure 4.3o Mass Transit

source: The Planning Center, 2006





The existing topography of the western part of Planning Area 1 gently slopes to the west towards Western Avenue, with the remainder of Planning Area 1 and all of Planning Area 2 sloping moderately in a southerly direction toward the Dominguez Channel. Planning Area 3 is graded and partially paved. Planning Areas 4 and 5 slope moderately in a northerly direction towards Artesia Boulevard. The quantity of cut or fill may vary as final grading plans are developed and the need for clean soil for the sump site is determined.

The primary objectives of the grading concept are to: (a) establish road grades consistent with those of the existing City streets around the site; (b) provide stable development pads for building structures; and (c) accommodate a drainage system that meets standards of the Los Angeles County Flood Control District.

4.5 PUBLIC FACILITIES/UTILITIES PLAN

The project benefits from the ability for proposed utility lines to connect to existing infrastructure facilities. The sewage, water, drainage, and solid waste plans are designed to serve the proposed development within the Artesia Corridor Specific Plan. The following is a summary of these infrastructure plans, as well as a description of the energy and utilities serving the project.

4.5.1 Sewage

The Artesia Corridor Specific Plan is served by the City of Gardena Department of Public Works, which maintains the sewer collection and distribution systems located throughout the city. The project is located within the County Sanitation Districts of Los Angeles County (CSDLAC) District No. 5, and so wastewater generated by the site will be treated at the Joint Water Pollution Control Plant located in the City of Carson.

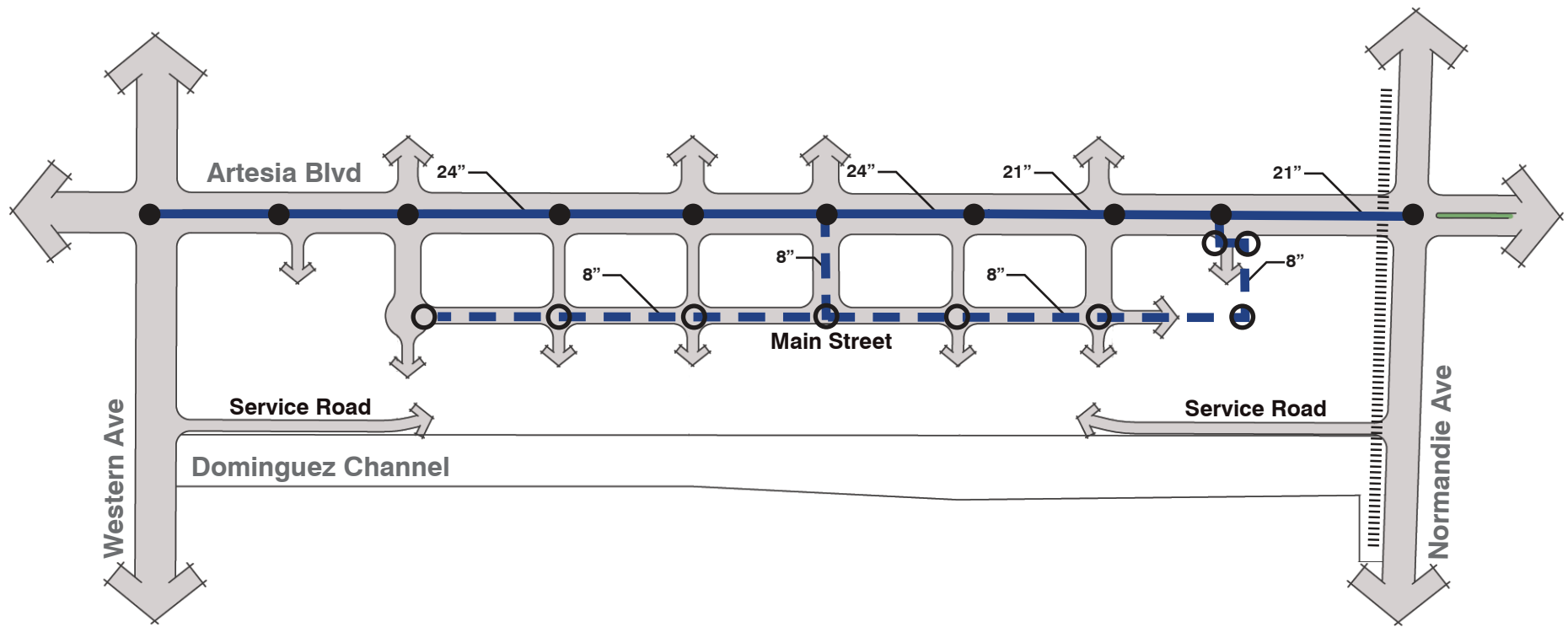
There is an existing sewer trunk line, the CSDLAC's Gardena Pump Trunk Sewer, that runs along Artesia Boulevard. Wastewater flow from the project will discharge directly into this line, which varies in size from 18" to 24" in diameter. Proposed sewer lines include the addition of an 8" sewer line that runs the length of the site parallel to Artesia Boulevard, as well as two 8" sewer lines that will connect to the existing sewer trunk line in Artesia Boulevard. The existing and proposed sewer lines are delineated in Figure 4.5a. Sewer improvements also include the addition of nine sewer manholes on the site.

Both the trunk sewer line along Artesia Boulevard and the Carson Treatment Plant facility can accommodate the expected increase in average wastewater flow from the Artesia Corridor Specific Plan at buildout. Uses within the Specific Plan would not exceed wastewater treatment requirements of the Regional Water Quality Control Board (RWQCB). Any wastewater utilities and service system upgrades deemed necessary by the City of Gardena Department of Public Works for the purpose of serving the uses within the Specific Plan area would be at the cost of the individual project applicants.







4.5.2 Water

Water service is provided by the Golden State Water Company (GSWC). The water delivered by GSWC is a blend of groundwater from the West Coast Groundwater Basin and imported water from the Colorado River Aqueduct and the State Water Project in northern California.

Existing water lines that serve the project include 12" lines along Western and Normandie Avenues, and a water line in Artesia Boulevard that varies in size from 10" at Western Avenue to 12" at Normandie Avenue. Proposed water lines include an 8" water line that runs through the site parallel to Artesia Boulevard, and three additional 8" water lines that connect this line to the existing water line along Artesia Boulevard. The existing and proposed water lines are delineated in Figure 4.5b.



Sewer Plan Legend

-  Roadways
-  Union Pacific Railroad
-  Existing Sewer Line
-  Existing Sewer Manhole
-  Proposed Sewer Line
-  Proposed Sewer Manhole

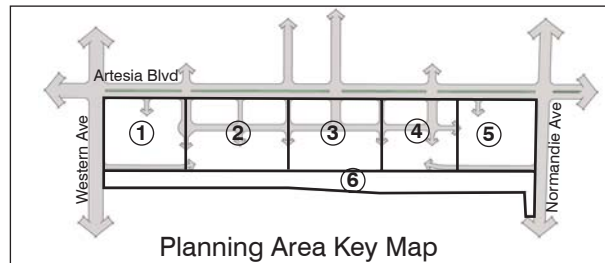
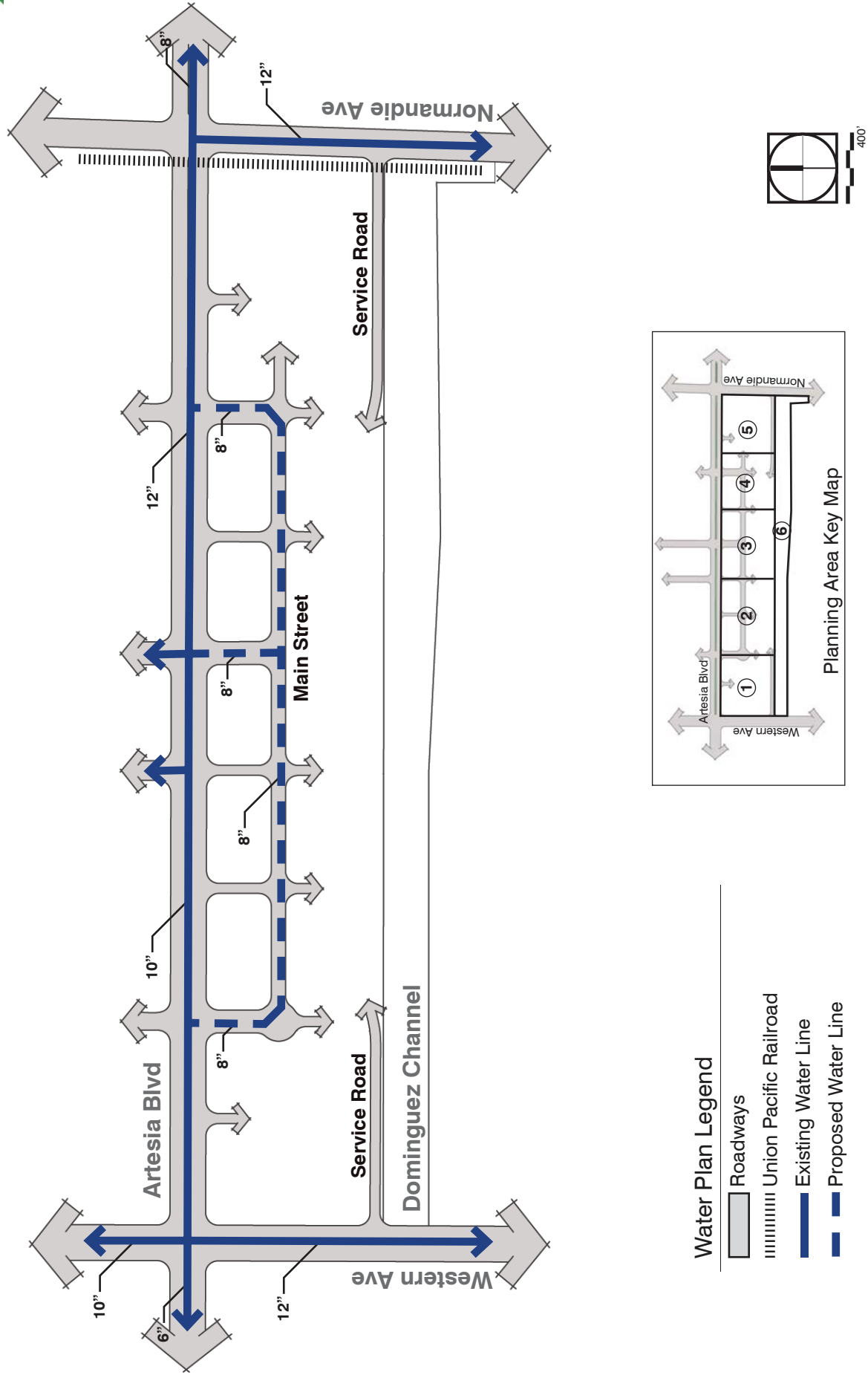


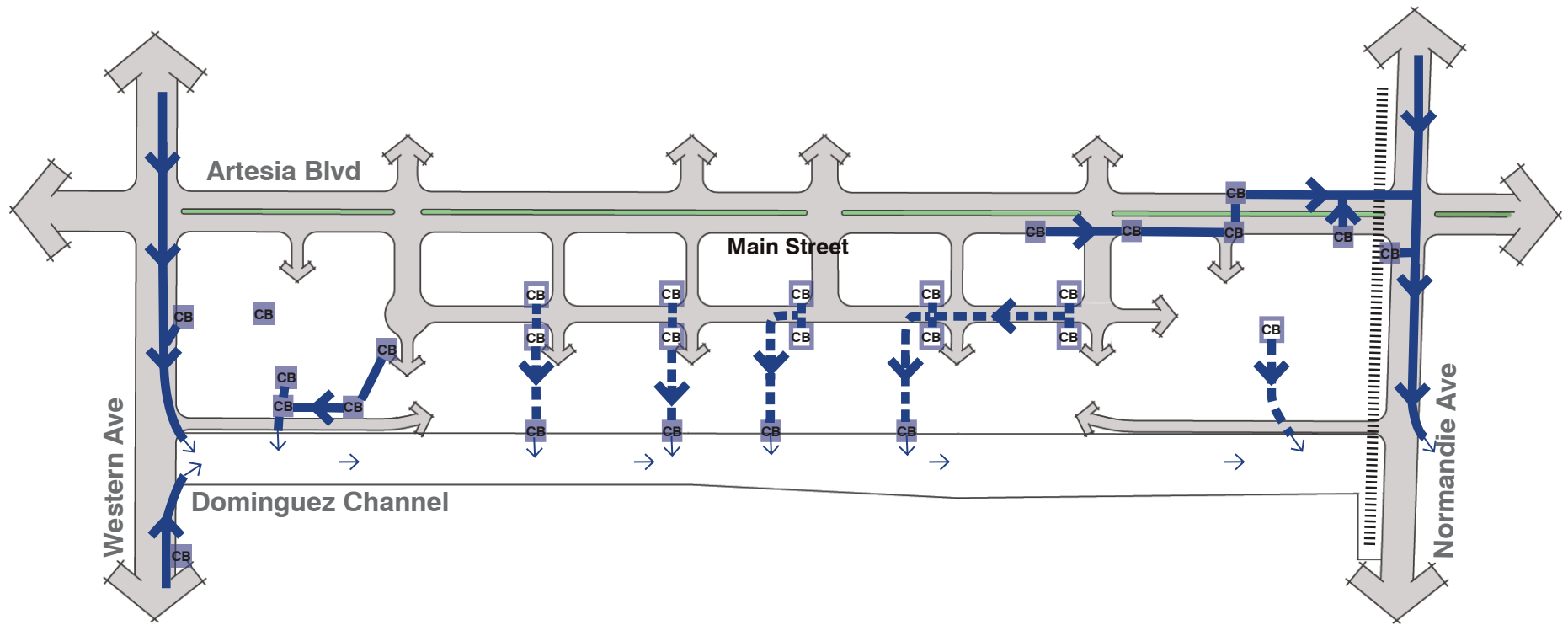
Figure 4.5a Sewer Plan





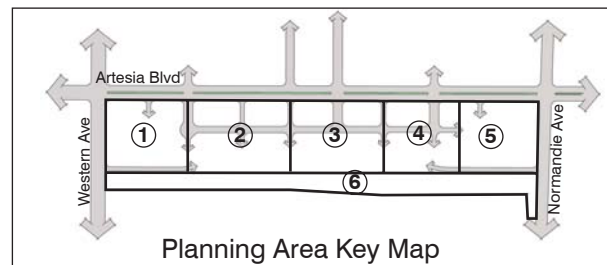
source: MDS Consulting, 2006

Figure 4.5b Water Plan



Drainage Plan Legend

- Roadways
- Union Pacific Railroad
- Existing Storm Drain Line
- Existing Catch Basin (Inlet)
- Proposed Storm Drain
- Proposed Catch Basin



source: MDS Consulting, 2006

Figure 4.5c Drainage Plan





The total estimated water consumption for the Specific Plan at buildout is 230,500 gallons per day. The highest demand for fire protection to the proposed land uses is 4,000 gallons per minute for four hours at 20 pounds per square inch residual. GSWC has indicated the ability to meet the increase in water demand and the water requirements for fire protection with sufficient water supply and facilities.

4.5.3 Drainage

Existing drainage for the Artesia Corridor Specific Plan includes a combination of sheet flow, concentrated drainage swales, localized catch basin inlets, and storm drain pipes. All storm runoff eventually reaches the Dominguez Channel, which flows easterly and southerly. Urban runoff from the site is collected and discharged to the adjacent municipal storm drains and to the Dominguez Channel. The project is outside of the 100-year flood plain adjacent to the Dominguez Channel, and therefore is not subject to flooding.

To stay within the maximum permitted peak runoff rate of 2.0 cubic feet per second per acre identified by the Los Angeles County Flood Control District, onsite detention of storm drain flows may be required. Onsite detention of storm flows may take the form of detention basins, depressed parking areas or streets to act as detention areas, underground storage in vaults, or enlarged storm drain pipes. A conceptual internal storm drain system for the Artesia Corridor Specific Plan is shown in Figure 4.5c. Drainage, flood control structures, and improvements are subject to review and approval by the City of Gardena Public Works Department and the Los Angeles County Department of Public Works.

4.5.4 Solid waste

The Waste Resources of Gardena provides solid waste collection services for the development within the Artesia Corridor Specific Plan. The Waste Resources of Gardena transports waste from the City of Gardena to either the Southeast Resources Recovery Facility Transformation Station in the City of Long Beach or to the Carson Transfer Station and Materials Recovery Facility in the City of Carson. Solid waste that is not recycled at these locations is sent to the Puente Hills Landfill No. 6, a Class III landfill in the City of Whittier, owned and operated by the Sanitation Districts of Los Angeles County (Districts).

The Districts has plans to transport solid waste to remote landfill facilities in either Imperial County or Riverside County. This solid waste-by-rail program is estimated to be operational in 2009. The Puente Hills Materials Recovery Facility is an initial departure point of residual waste for transport.

At buildout, the Artesia Corridor Specific Plan is estimated to generate approximately 3,656 tons of solid waste per year, and dispose of approximately 1,865 tons of solid waste per year. These amounts will be supported by the current solid waste collection. Property owners are required to implement existing and future waste reduction programs in conformance with the City of Gardena's Source Reduction and Recycling Element.

4.5.5 Public Services

The project site and surrounding area is developed and afforded all municipal services. Fire protection and emergency medical response services for Gardena are provided by the Los Angeles County Fire Department (LACFD). The LACFD operates two fire stations within the City of Gardena. The LACFD Fire Station that will serve the Artesia Corridor Specific Plan is Fire Station 158, located approximately 1.3 miles away at 1650 West 162nd Street. Because of the strategic location of the station, response times for emergency calls are generally within the four minute goal established by the LACFD.

The highest fire protection demand for existing land uses is 4,000 gallons per minute for four hours at 20 psi residual. The Golden State Water Company (GSWC) currently operates water lines along Western



Avenue, Artesia Boulevard, and Normandie Avenue, all bordering the Artesia Corridor Specific Plan project. The GSWC has indicated that the system has adequate capacity to meet the fire protection demand dictated by the LACFD.

Educational services are provided to Gardena by the Los Angeles Unified School District (LAUSD). The project site is within the attendance boundaries of three LAUSD schools: 186th Street Elementary School, Peary Middle School, and Gardena High School. The 186th Street Elementary and Gardena High schools are currently operating within their design capacities.

4.6 RECREATION AND OPEN SPACE PLAN

Similar to many cities in Los Angeles County, Gardena is a totally developed community and has limited parks and recreation resources. It is therefore critical that development within the Artesia Corridor Specific Plan contribute adequate recreation and open space areas for both future residents and consumers. This need is balanced by the unique mixed-use commercial nature of the site, the success of which allows for little opportunity to create an open space adequate for parkland use. Thus, the Recreation and Open Space Plan shown in Figure 4.6a provides guidelines for the establishment of parkland directly adjacent to the Artesia Corridor Specific Plan along the Dominguez Channel in addition to the network of public open spaces within the project site.

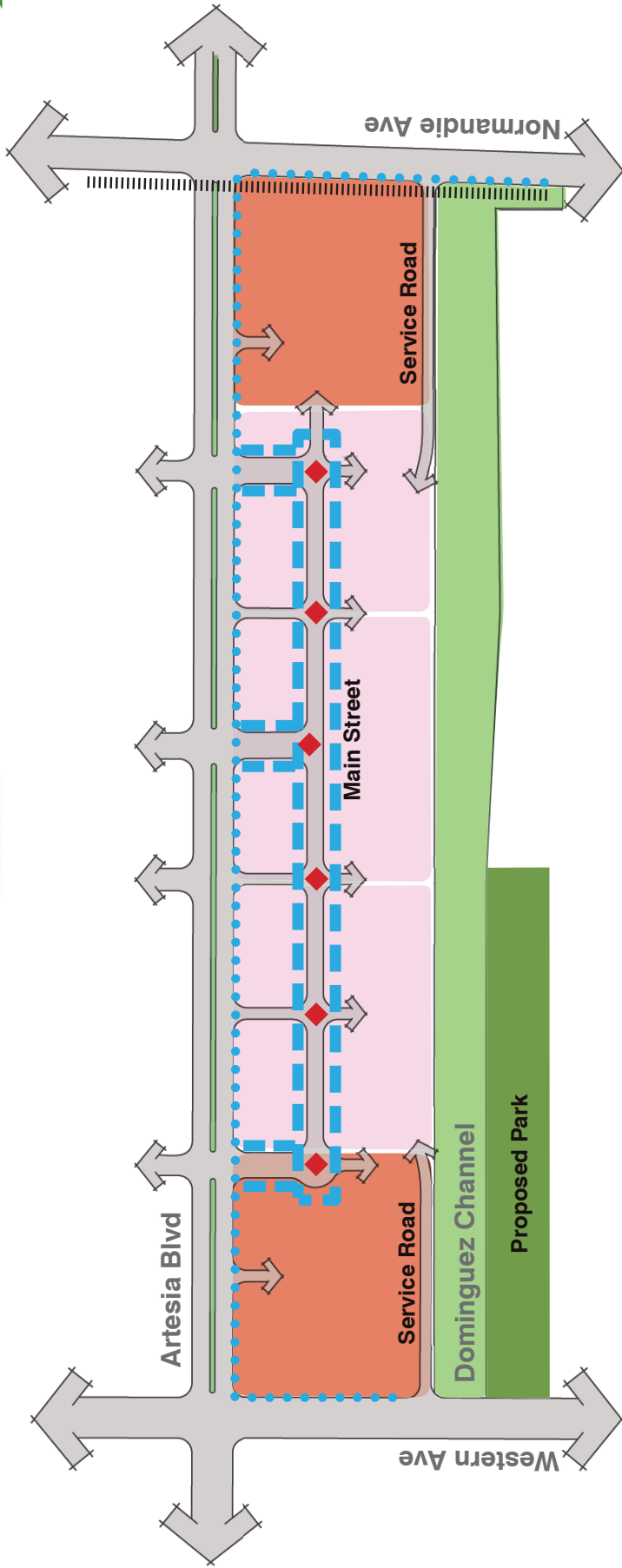


The Artesia Corridor Specific Plan allows for the integration of commercial uses with high density residential and live/work development in conjunction with public open spaces. To take advantage of the commercial potential within the Artesia Corridor Specific Plan and provide residential opportunities, the site layout is more conducive to the establishment of a series of smaller, more intimate open spaces.



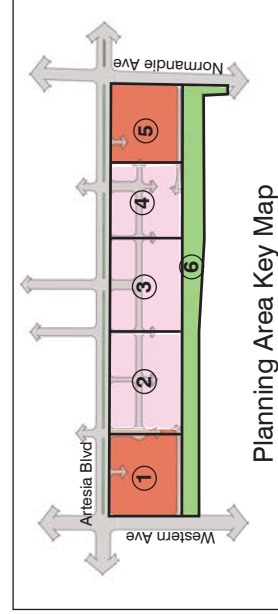
The pedestrian spaces are focused along the main street within the project, establishing a unique environment for people to gather. Public open spaces such as walkways, multi purpose paths, and plazas provide spaces for window shopping, eating or just enjoying the atmosphere. The Development Standards section of this Plan identifies a minimum requirement of open space for the residential and mixed-use areas and public plaza space for the commercial areas, and prescribes dimensions for walkways and pathways throughout the project. Also, Section 5.2 outlines specific standards for the use of these spaces. The Design Guidelines establish design parameters for these areas, which include seating areas, shade trees, and water features.

Because the Artesia Corridor Specific Plan will not provide a parkland space directly on the site, in-lieu fees must be paid as dictated by the City of Gardena Municipal Code (2006), Section 17.20.030. Buildout of the Specific Plan will result in the addition of 885 residents to the City of Gardena, which translates to a total of 2.7 acres of parkland required, according to the Quimby standard of 3.0 acres per 1,000



Recreation and Open Space Plan Legend

- Roadways
- External Pedestrian Access
- Internal Pedestrian Access
- Pedestrian Plaza
- Park



Planning Area Key Map



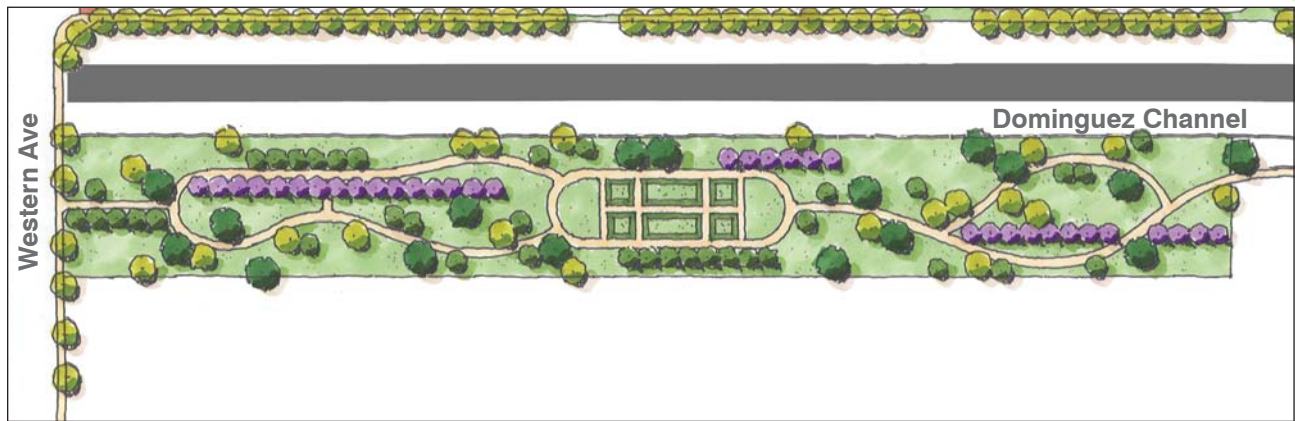


Figure 4.6b Illustrative Park Plan

source: The Planning Center

persons. The Specific Plan recommends that the in-lieu fees required for park requirements be used towards the creation of parkland directly across the Dominguez Channel.

The area south of the Channel, identified in Figure 4.6a, is the recommended site for potential parkland and covers approximately 4.3 acres. The site is located within an Edison easement, where generally the conversion of easement to open space would be difficult. However, with the payment of in-lieu fees, the fees would directly transfer to improve the site for parkland. This would allow for passive parkland used for relaxation, exercise, and other activities. Potential pedestrian and/or vehicular bridges could be constructed across the Channel to connect the project to the parkland area. A conceptual illustration is provided in Figure 4.6b.

The eventual transition of the easement adjacent to the channel to an open space amenity is envisioned by the Los Angeles River Project. The potential for future trails to follow the Dominguez Channel creates the opportunity to connect the parkland to surrounding residents.

4.7 LANDSCAPE PLAN

The landscape design will establish an identity and theme for the project and will act as an overall unifying element, transcending parcel boundaries and defining open space areas. The landscape plan provides guidelines for the treatment of areas within the Artesia Corridor Specific Plan, including parkways, medians, development edges, project entries, and open space areas. The landscaping theme is based upon eastern influences, drawing inspiration from Marukai's design quality and existing presence on site. The selection of plants may include, but are not limited to, the use of bamboo, palm trees, pine trees, and assorted hedges. Rock gardens and water features may also be used to enhance landscape elements.

The Landscape Plan is organized by the hierarchy of roadways, including points of interest such as enhanced gateways, pedestrian plazas, parking areas, and the proposed park. Generally, landscaping will be more urban in nature at the core of the Artesia Corridor project, with the use of



Example of landscaping along external streets.



Note: Placement of pedestrian plazas and landscaping elements are conceptual.

source: The Planning Center, 2006

Figure 4.7 Landscape Plan



planters and tree grates along the main street and throughout pedestrian plazas. Landscaping along the edges of the project will soften, emphasizing green hedges and the formal planting of trees. The location of these areas are identified in Figure 4.7a.

4.7.1 External Streets

Landscape along major streets (Artesia Boulevard, Western Avenue, and Normandie Avenue) should be consistent and formal in nature, distinct from the surrounding streets to emphasize the throughway adjacent to the site. Landscaping along these external edges will be more intense than landscaping within the project, but should also preserve and enhance strategic view corridors into the project to take advantage of frontage along these major traffic corridors.

Two conditions along the external streets present further landscape opportunities for the project: the median along Artesia Boulevard and the Union Pacific Railroad easement along Normandie Avenue. The median along Artesia Boulevard provides an opportunity to exhibit a landscape presence along the major roadway adjacent to the Artesia Corridor project. Planting along the median should be formal in nature, consistent with landscape elements within the project, and distinctive from surrounding streets. As well, the Union Pacific Railroad easement allows space for more intense landscaping, informal in nature, and closely planted to buffer uses, screen the railroad and enhance the appearance of Normandie Avenue. Additional median improvements could be extended to Western Avenue (partially located within the City of Torrance) and Normandie Avenue to improve the image of the City and project.



Tall landscape elements draw attention to entries and points of interest within the project.

4.7.2 Enhanced Gateways

The landscaping for enhanced gateways along Artesia Boulevard should accent monument entry signs signaling entrance to the City of Gardena and to the Artesia Corridor project. Vertical plant elements such as palm trees help to draw attention to these areas to passing motorists. Landscaping should coordinate and augment signage at these areas, as well as complement landscape elements within the project.



Example of landscaped setback for intersection areas.

4.7.3 Dominguez Channel

The landscape design adjacent to the Dominguez Channel may be informal, with a concentration of low shrubs and hedges to screen walls along this easement. Low shrubs in conjunction with canopy trees should be placed deliberately to



Use of potted plants and water features create focal points along the main street.

screen undesirable views, but should also open up in strategic areas to the open space created by the easement.

4.7.4 Service Road

Landscaping along the service road should aim to screen utility facilities and adjacent building facades with shrubs, trees, and climbing vines where appropriate. Landscape design should be informal, and plantings should appear more dense around intersection areas.

4.7.5 Major Entries

The landscape design for major entries should consist of the highest quality of landscape design that works in conjunction with themed signage, lighting, and hardscape elements. Landscape should accent entry monuments fronting Artesia Boulevard, with a formal tree pattern along the length of each street to create a sense of entrance and arrival. Taller trees such as palm trees shall be placed at the intersections of Artesia Boulevard and the Major Entries to emphasize the entryway.

4.7.6 Right In/Out

Landscaping for right in/out entries shall be formal and more subtle in nature. Landscape design may become more dense at intersections, but the overall scale should be less than that of a Major Entry. Yet the same level of quality and attention to detail must be maintained.

4.7.7 Main Street

Landscape along the Main Street will be urban in nature and emphasize hardscape elements such as special paving on sidewalks and crosswalks, bulb-out curbs at intersections, public art, seating areas, treated newspaper racks, themed lighting standard, shade structures, and other street furniture. Trees shall be planted with tree grates or planters, enhanced with the use of potted plants. A regular pattern of tree plantings along the length of the Main Street will emphasize the significance of the thoroughway, and allow for appropriate view corridors to adjacent businesses. This formal planting may be interrupted at the juncture of pedestrian plazas to indicate these areas as focal points along the street. A consistent plant palette should be used along the Main Street to add harmony to the streetscape.

4.7.8 Pedestrian Plazas

The plant palette for pedestrian plazas along the Main Street should be consistent and in coordination with the Main Street landscaping. As well, landscape elements include the use of tree grates, planters, and potted plants to create an urban feel. However, these landscape elements will be more asymmetrical in arrangement, adding interest to the pedestrian space and creating distinction from the regular tree plantings along the Main Street. Landscape design should emphasize these areas as focal points with the use of accent features such as rock gardens, public art, and water features. Shade structures and street furniture should be highlighted in order to promote use and create pedestrian friendly spaces.



4.7.9 Alley

Due to the minimal space available for planting along the alleys within the project, an emphasis is placed on climbing vines and small potted plants between garage spaces along these thoroughways. Creativity in incorporating different landscape elements along alleys is encouraged to create an interesting streetscape. For example, where there is insufficient space for either a vine or plant, hanging plants may be an acceptable landscape element.



Rock gardens add interest to pedestrian plazas.

4.7.10 Parking areas

One tree for every four spaces shall be planted in parking areas. These trees may be clustered at key locations in the parking areas or spread throughout the parking areas. The placement and use of trees and other landscaping should emphasize the creation of shaded parking spaces at full maturity. Landscape islands may narrow and widen to accommodate the tree plantings. Where there is a pedestrian passageway through a parking area, formal landscaping should be used to highlight these areas and direct pedestrians towards their destinations.



Potted plants and climbing vines add a green element to alleys.

4.7.11 Proposed Park

Landscaping for the proposed park should reflect the plant palette used within the Artesia Corridor project. Additional native plants may also be used to augment the plant palette and promote variety. Plantings should be clustered together for a natural feel, with designated open areas to encourage more active recreational activities. Dense plantings along park walkways should provide a rhythm of concealing and revealing views in and out of open space areas.



Example of park spaces.



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5. DEVELOPMENT REGULATIONS AND REQUIREMENTS

This chapter contains the regulations, requirements and by-laws by which development must abide, indicated by the use of the word “shall.” These regulations are mandatory and cover general development standards, recreation and open space requirements, parking standards, nonconformities, lighting standards, sign programs, maintenance standards, and other standards for accessory structures, walls, and fences. Provisions within these standards may also use the word “should,” in which case the standard is encouraged but not mandatory.

5.1 DEVELOPMENT STANDARDS

The development standards set forth for commercial, residential, and mixed-use development are intended to provide for flexibility in site design as well as methods to integrate commercial and residential uses within the Artesia Corridor Specific Plan area. These development standards prescribe the minimum standards for development and typically concern topics such as permitted uses, density, building and property dimensions, and the quantity of parking and landscaping.

5.1.1 Permitted Uses

Each land use designation is defined in precise terms to ensure that the range of permitted and conditionally permitted uses respects the intent of the plan and the conditions encountered in each portion of the site. The following table outlines the permitted uses (P), conditionally permitted uses (C), uses requiring a home occupancy permit (H), and temporary uses (T) allowed to develop within the Artesia Corridor Specific Plan by land use designation. This list was derived from existing zoning regulations to allow greater or lesser flexibility in some areas of use and to customize land use requirements in response to site characteristics, potentials and limitations. This tailored use list is specifically aimed at stimulating investment on this site to generate a land use pattern of higher value and quality than has prevailed in the past. The table also lists prohibited uses to avoid potential conflicts between uses within, and adjacent to, the site.

Those uses not specifically listed in the table are subject to review based on the consistency within the purpose and intent of the land use categories and planning areas and are subject to the approval of the Community Development Director.

| Table 5-1a Permitted Uses | | | | | |
|---|--|------------|----------------------|----------------------|--------|
| (P) Permitted by Right (C) Conditional Use Permit (H) Home Occupancy Permit (T) Temporary Use Permit (X) Not Permitted (N/A) Not Applicable | | | | | |
| Use Category | Typically Permitted Uses | Commercial | Mixed Use | | Public |
| | | | north of main street | south of main street | |
| RESIDENTIAL | | | | | |
| residential | multi family dwellings | X | X | P | X |
| | single family attached dwellings | X | X | P | X |
| COMMERCIAL | | | | | |
| food sales and service | confectionery, candy stores, retail bakery | P | P | P | X |
| | neighborhood markets | P | P | P | X |
| | restaurants, coffee shops, cafeterias | P | P | P | X |
| | restaurants with drive-through facilities | C | C | X | X |
| | supermarkets | S | X | X | X |



**Table 5-1a
Permitted Uses**

| (P) Permitted by Right (C) Conditional Use Permit (H) Home Occupancy Permit (T) Temporary Use Permit (X) Not Permitted (N/A) Not Applicable | | | | | |
|---|--|------------|----------------------|----------------------|--------|
| Use Category | Typically Permitted Uses | Commercial | Mixed Use | | Public |
| | | | north of main street | south of main street | |
| health services | medical, dental, optical offices | P | C | C | X |
| | pharmacies | P | C | C | X |
| | day spas and health clubs ≤10,000 square feet | P | P | C | X |
| office | administrative, financial, real estate, bank, and professional offices | C | C | C | X |
| public assembly | auditoriums | C | C | C | X |
| | bowling alleys | P | P | C | X |
| | lodges and meeting halls | C | C | C | X |
| | museums | C | C | C | X |
| | skating rinks | P | P | C | X |
| | theaters (motion picture or otherwise) | P | P | C | X |
| retail sales and service | antique stores | P | P | P | X |
| | art galleries | P | P | P | X |
| | automobile service stations | C | X | X | X |
| | boutique vintage shop (1) | C | C | C | X |
| | barber shops and beauty shops | P | P | C | X |
| | blueprinting and photostating | P | C | C | X |
| | book and stationary stores | P | P | P | X |
| | clothing stores ≤5,000 square feet | P | P | P | X |
| | clothing stores >5,000 square feet | C | C | C | X |
| | department and furniture stores ≤5,000 square feet | P | P | X | X |
| | department and furniture stores >5,000 square feet | C | C | X | X |
| | dry cleaning service | P | P | P | X |
| | florists and plant shops | P | P | P | X |
| | furniture upholstery shops | C | C | C | X |
| | gift shops | P | P | P | X |
| | hardware and paint stores ≤5,000 square feet | P | P | C | X |
| | hardware and paint stores >5,000 square feet | C | C | C | X |
| | hotels and motels | C | X | X | X |
| | interior decorating shops | P | P | P | X |
| | jewelry stores | P | P | P | X |
| | music and vocal instruction and dancing academies | P | C | C | X |
| | newsstands (non-coin operated) | P | P | P | X |
| | outdoor sales | T | T | T | X |
| | pet shops | P | C | C | X |
| | shoe sales | P | P | P | X |
| | tailor and dress shops | P | P | P | X |
| | television, radio, and home appliance stores | P | P | X | X |



**Table 5-1a
Permitted Uses**

| (P) Permitted by Right (C) Conditional Use Permit (H) Home Occupancy Permit (T) Temporary Use Permit (X) Not Permitted (N/A) Not Applicable | | | | | |
|---|--|------------|----------------------|----------------------|--------|
| Use Category | Typically Permitted Uses | Commercial | Mixed Use | | Public |
| | | | north of main street | south of main street | |
| studios | artist's and photographer's studios | P | P | P | X |
| public facilities | fire and police stations | P | P | X | X |
| | libraries | P | P | P | X |
| | small post offices | P | P | P | X |
| other | businesses operating between 10:00 P.M. and 6:00 A.M. | C | C | C | X |
| MIXED-USE | | | | | |
| mixed-use | vertically or horizontally attached buildings with two or more different uses (e.g., commercial and residential) but excluding live/work units unless attached to a separate commercial or residential building. | X | C | C | X |
| LIVE/WORK (2) | | | | | |
| food sales and service | restaurants and eating establishments | N/A | C | C | N/A |
| professional, administrative, and business uses | architect/engineer/planner | N/A | H | H | N/A |
| | attorney | N/A | H | H | N/A |
| | billing service provider | N/A | H | H | N/A |
| | consulting and business services | N/A | H | H | N/A |
| | dance teacher | N/A | H | H | N/A |
| | estate planner | N/A | H | H | N/A |
| | income tax service/accounting | N/A | H | H | N/A |
| | interior decorator | N/A | H | H | N/A |
| | internet or web-oriented business | N/A | H | H | N/A |
| | kitchen and bath designer | N/A | H | H | N/A |
| | music teacher | N/A | H | H | N/A |
| | photography studio/portrait/bridal services | N/A | H | H | N/A |
| | planning consultant | N/A | H | H | N/A |
| | public relations consultant | N/A | H | H | N/A |
| | real estate developer/specialty contractor | N/A | H | H | N/A |
| | secretary/communication service provider | N/A | H | H | N/A |
| retail sales and service | building contractor office | N/A | H | H | N/A |
| | business consulting | N/A | H | H | N/A |
| | catering services | N/A | C | C | N/A |
| | child care facilities | N/A | C | C | N/A |
| | electronic/computer equipment repair | N/A | H | H | N/A |
| | flower shops | N/A | H | H | N/A |
| | landscaping office | N/A | H | H | N/A |
| | pick-up and delivery services | N/A | C | C | N/A |
| | printing and lithography facilities | N/A | C | C | N/A |
| studios | ceramic and pottery studios | N/A | H | H | N/A |
| | copywriter studios | N/A | H | H | N/A |
| | commercially operated professional studios | N/A | C | C | N/A |



**Table 5-1a
Permitted Uses**

| (P) Permitted by Right (C) Conditional Use Permit (H) Home Occupancy Permit (T) Temporary Use Permit (X) Not Permitted (N/A) Not Applicable | | | | | |
|---|--|------------|----------------------|----------------------|--------|
| Use Category | Typically Permitted Uses | Commercial | Mixed Use | | Public |
| | | | north of main street | south of main street | |
| studios continued | fine art studios, museums, galleries | N/A | H | H | N/A |
| | photography studios | N/A | H | H | N/A |
| | video producer studios | N/A | H | H | N/A |
| public facilities | government buildings | N/A | H | H | N/A |
| other | businesses operating between 10:00 P.M. and 6:00 A.M. | N/A | C | C | N/A |
| ACCESSORY USES | | | | | |
| parking | parking lot/structure, shared parking facilities | P | P | P | P |
| public or private recreational facilities | parks, playgrounds, swimming pool, tennis court, skating rink, bike paths, jogging trails | P | P | P | P |
| passenger station | bus station, rail station, taxi stand | P | P | P | X |
| alcoholic beverage sales and service | in conjunction with a supermarket, department store, variety store, drug store, bar, billiards, restaurant, theatre | C | C | C | X |
| auto service station | car wash facilities | C | C | C | X |
| communication and utilities | stealthed transformer, transmitter, receiver, repeater station, or wireless signal tower | C | C | C | C |
| other | uses customarily incidental to retail businesses or accessory buildings when located on the same lot, provided there shall be no manufacturing, assembling, compounding, processing or treatment of products | C | C | C | X |
| PROHIBITED USES | | | | | |
| residential | lodging houses, licensed care homes, fraternity and sorority house | X | X | X | X |
| | single family detached dwellings | X | X | X | X |
| retail and industrial | adult-oriented businesses (3) | X | X | X | X |
| | check-cashing, payday loan service | X | X | X | X |
| | industrial uses | X | X | X | X |
| | laundromats | X | X | X | X |
| | auto dealerships, auto repair, auto parts sales | X | X | X | X |
| | off-site advertising | X | X | X | X |
| | storage, wholesaling, warehousing | X | X | X | X |
| | uses that generate excessive vibrations, noise, heat, or smells | X | X | X | X |
| | uses that use caustic chemicals on site (other than those used in copy machines), hazardous activities, such as, but not limited to, welding, open flame, or storage of flammable liquids | X | X | X | X |

NOTES FOR TABLE 5-1a:

- See Chapter 18.32 of the City of Gardena Municipal Code, 2006 (GMC).
- Covenants, conditions and restrictions for individual live/work projects may further restrict and prohibit uses, but shall not be more permissive. The buildings for live/work uses requiring a Home Occupancy Permit (see Section 8.2.5) shall be processed through the Administrative Development Review process (see Section 8.2.3).
- As defined by Section 18.62.020G of the GMC.



5.1.2 General Development Standards

Development standards control the building envelopes for the proposed commercial, residential, service and entertainment uses. These regulations have been designed to provide for flexibility in site design while ensuring a consistent and coordinated built environment for the Artesia Corridor.

| Table 5-1b General Development Standards | | | | |
|--|---|------------------------------|------------------------------|------------------------------|
| TOPIC | BUILDING USE | | | |
| DENSITY/INTENSITY | Commercial | Residential | Live/Work | Mixed-Use |
| Density | 15 du/ac max. overall (1) | | | |
| Floor Area Ratio (FAR) | 0.50 max. overall (2) | | | |
| Max. Percentage of Nonresidential Square Footage | n/a | n/a | 40% (3) | n/a |
| Unit Size (4) | n/a | 800 sq. feet min. | 800 sq. feet min. | 800 sq. feet min. |
| Unit Width (5) | n/a | 10 feet min. 15 foot avg. | 10 feet min. 15 foot avg. | 10 feet min. 15 foot avg. |
| Project Size | A Conceptual Master Site Plan is required for each planning area (6) | | | |
| BUILDING HEIGHT | Commercial | Residential | Live/Work | Mixed-Use |
| General Standards (Figures 5.1d-e) | | | | |
| base building | 40 feet max. | 40 feet max. | 40 feet max. | 40 feet max. |
| with architectural projections (7) | 45 feet max. | 45 feet max. | 45 feet max. | 45 feet max. |
| With Additional Features (8) (Figure 5.1f) | | | | |
| base building | n/a | 65 feet max. | 65 feet max. | 65 feet max. |
| with architectural projections (7) | n/a | 75 feet max. | 75 feet max. | 75 feet max. |
| BUILDING SETBACKS (9) | Commercial | Residential | Live/Work | Mixed-Use |
| Internal Building to Building (Figures 5.1a-c) | | | | |
| Commercial >20,000 sq. feet to: | 15/20 feet min. (10) | 20 feet min. | 20 feet min. | 20 feet min. |
| Commercial ≤20,000 sq. feet to: | 15 feet min. | 15 feet min. | 15 feet min. | 15 feet min. |
| Residential to: | 15/20 feet min. (10) | 10 feet min. | 10 feet min. | 10 feet min. |
| Live/Work to: | 15/20 feet min. (10) | 10 feet min. | 10 feet min. | 10 feet min. |
| Mixed-Use to: | 15/20 feet min. (10) | 10 feet min. | 10 feet min. | 10 feet min. |
| Building >40 feet in height to: | 20 feet min. (11) | | | |
| Internal Building to Parking/Street | 6 feet min. from building to curb when adjacent to a right-in/out entrance; otherwise 10 feet min. from building to curb | | | |
| Perimeter Setbacks | | | | |
| Artesia Boulevard | 22 feet min. from building to curb | | | |
| Western Avenue | 16 feet min. from building to curb | | | |
| Normandie Avenue | 36 feet min. from building to railroad property line | | | |
| Dominguez Channel | 5 feet min. for from side of building to Channel property line 15 feet for front/rear of building to Channel property line | | | |
| ENCROACHMENTS/PROJECTIONS | Commercial | Residential | Live/Work | Mixed-Use |
| Encroachments/Projections | 5 or 8 feet max. (12) | | | |
| WALKWAYS/PATHWAYS | Commercial | Residential | Live/Work | Mixed-Use |
| Internal Walkways/Pathways | 4 feet min. sidewalk or path area clear from any obstructions | | | |

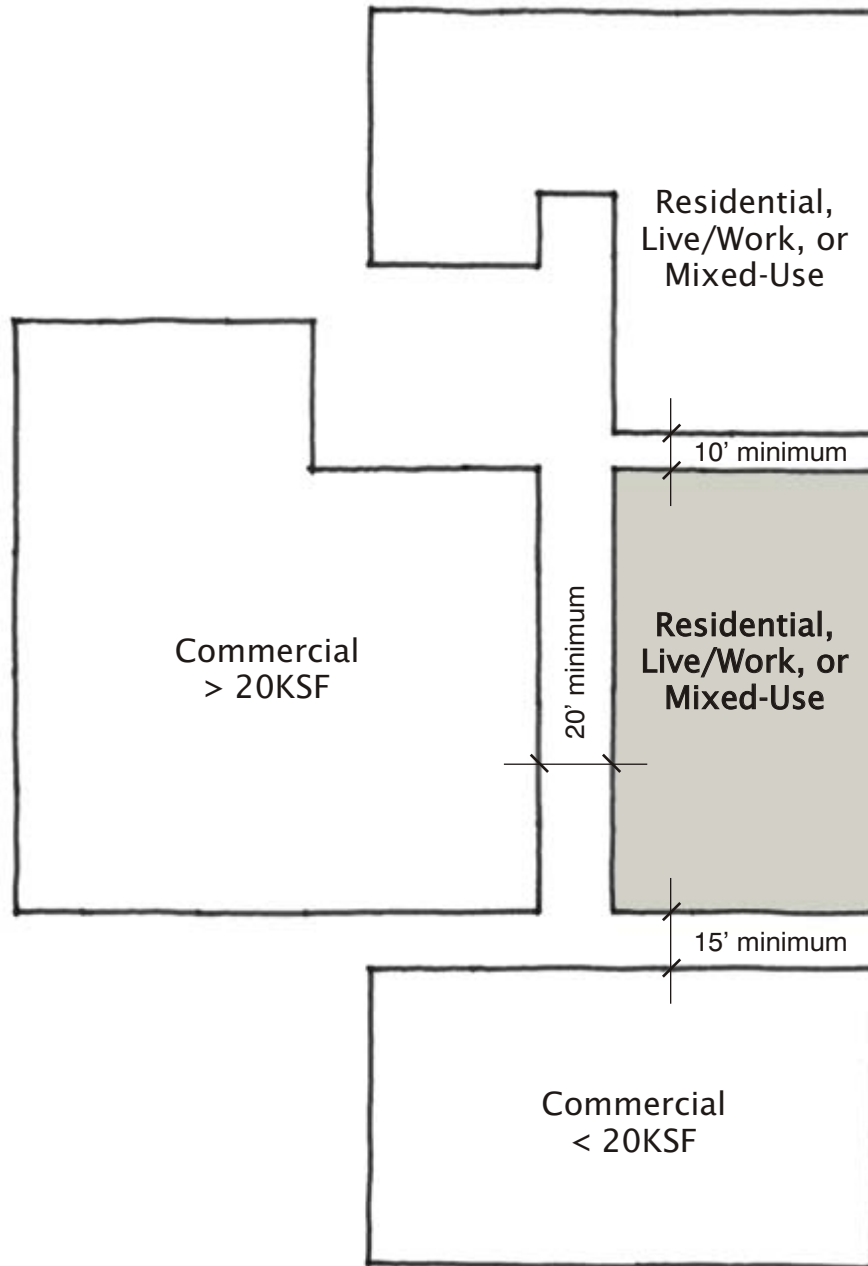


NOTES FOR TABLE 5-1b:

1. The Artesia Corridor Specific Plan permits a maximum of 300 units, 70 of which can be live/work, on the entire project site. The maximum density for the Artesia Corridor Specific Plan must not exceed 15 dwelling units per acre, as calculated by the total number of units divided by the number of acres within the specific plan (44 acres).
2. The maximum intensity of development within the Artesia Corridor Specific Plan must not exceed 0.50 FAR. The intensity is calculated as the total amount of square feet of building (regardless of use, but excluding any area devoted to parking), divided by the total number of square feet within the specific plan (approximately 1,921,000 [44 acres]).
3. The maximum ratio of office square footage to residential square footage is 0.40:1. This ratio ensures that the live/work space does not transition to a predominantly office use.
4. Smaller units are allowed with a Conditional Use Permit, but in no case shall a unit smaller than 450 square feet be approved. Studio units smaller than 800 square feet must only provide 1.5 covered parking space and 0.5 uncovered parking space per unit.
5. Residential and live/work units are permitted to be as narrow as 10 feet (as measured from the interior walls) for no more than 20 percent of the unit length. Unit widths must average at least 15 feet for the overall unit length. Parking areas for tandem spaces must be a minimum of 12 feet wide by 36 feet long for residential units and 12 feet wide by 40 feet long for live/work units.
6. Conceptual Master Plan procedures and content are governed Section 8.2.2.
7. Architectural projections are building elements (e.g., towers, cupolas) that are added to building faces to provide architectural interest without adding interior floor area. The maximum height of any architectural projection is 10 feet, unless otherwise approved by the Community Development Director.
8. The maximum height for any building north of Main Street shall be 40 feet (45 feet with projections). For buildings south of main street, the building height may increase to 65 feet (75 feet with projections) if features are provided in agreement with Table 5-1c. Figure 5.1d provides an illustration of building height standards. Buildings occupied solely by commercial uses are not permitted to exceed 40 feet (45 feet with projections) at any time.
9. All uses are allowed to be attached horizontally. Accordingly, the setback requirement for the building at the point of the shared wall is zero. If the attached uses are a mix of two different uses (live/work is considered one use), the building shall be considered a mixed-use building and shall be subject to the setback requirements as stated above. Setbacks are measured from the closest point of a building to the closest point of an adjacent building or the back of curb as appropriate.
10. Commercial buildings greater than 20,000 square feet must be set back a minimum of 20 feet from another commercial building that is greater than 20,000 square feet or any residential, live/work, or mixed-use building. Commercial buildings greater than 20,000 square feet must be set back a minimum of 15 feet from a commercial building that is 20,000 square feet or less or any residential, live/work, or mixed-use building.
11. Building height of 40 feet refers to base building height.
12. Outdoor dining, benches, outdoor displays, or any other ancillary uses as approved by the Community Development Director may encroach into the sidewalk area a maximum of 8 feet from the building frontage, so long at least 4 feet of unobstructed walkway is maintained. Private open spaces, architectural features, and cantilevered portions of buildings can encroach into a setback area a maximum of 5 feet, with at least 8 feet of headroom below.



BUILDING TO BUILDING SETBACKS FOR RESIDENTIAL, LIVE/WORK OR MIXED-USE BUILDINGS



source: The Planning Center, 2006

Figure 5.1a Building Setbacks for Residential, Live/Work, or Mixed-Use



BUILDING TO BUILDING SETBACKS FOR COMMERCIAL BUILDINGS > 20KSF

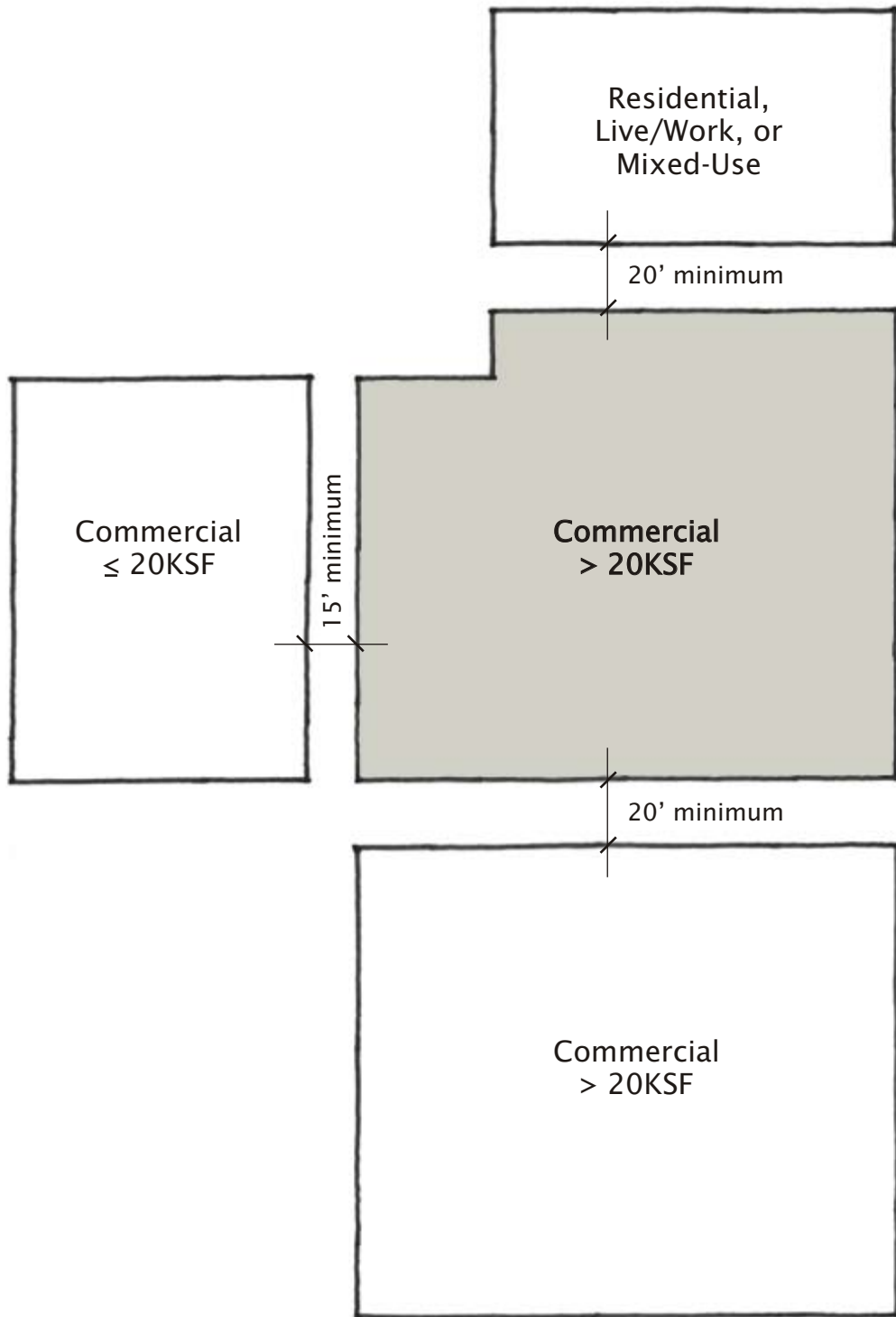


Figure 5.1b Building Setbacks for Commercial >20 KSF

source: The Planning Center, 2006



BUILDING TO BUILDING SETBACKS
FOR COMMERCIAL BUILDINGS ≤ 20 KSF

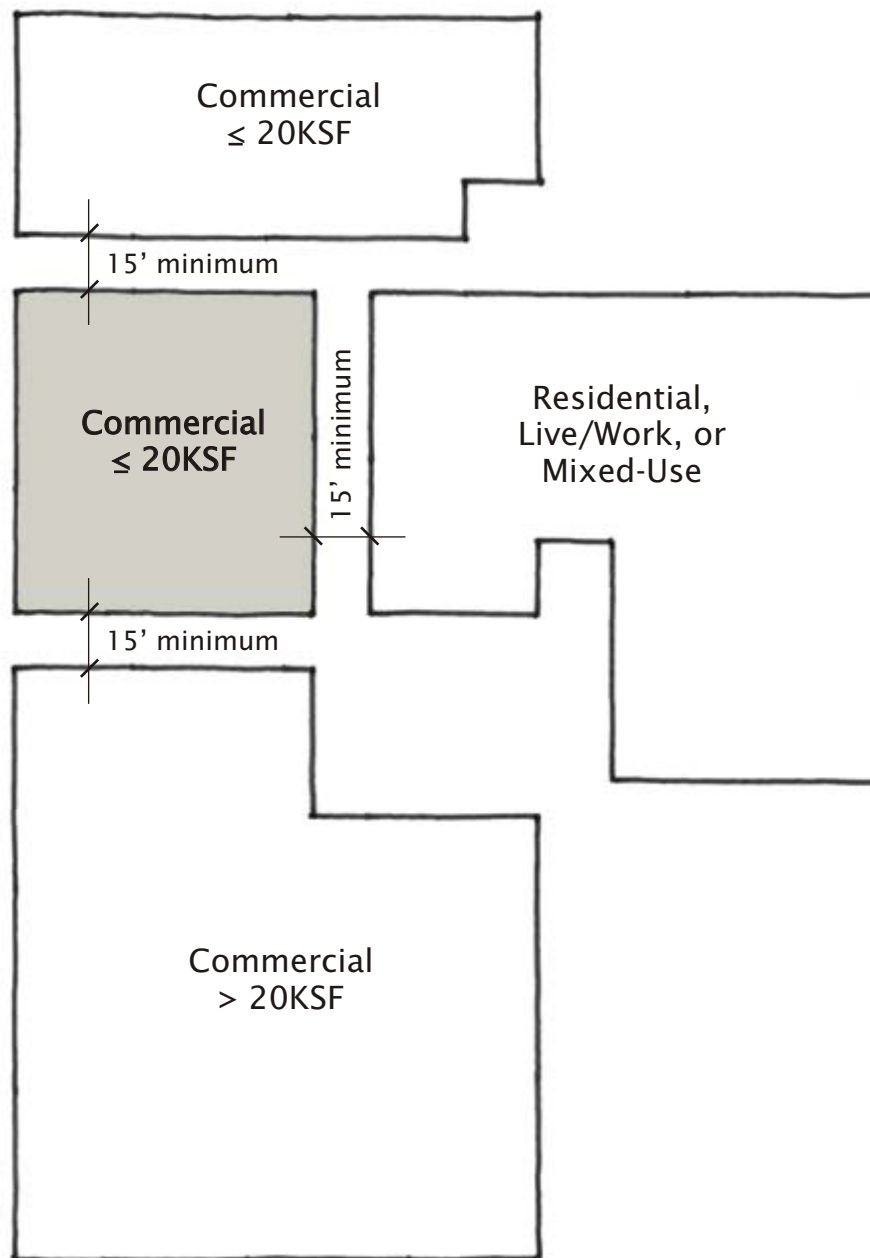


Figure 5.1c Building Setbacks for Commercial ≤ 20 KSF

source: The Planning Center, 2006



5.1.3 Live/Work Standards

Live/work units and buildings are subject to the following standards:

- A. Work on the premises of a live/work unit shall be limited to persons who live in the live/work unit. Living and working spaces shall not be rented or sold separately. The owner/occupant of a live/work unit shall notify the City of any change in use or occupancy. Any change of use or occupancy shall comply with the uses identified in this Specific Plan and will require a new Certificate of Occupancy. The square footage initially approved for live-work areas within a unit shall remain commercial in nature and shall not be converted to residential use with subsequent owners.
- B. Off-street loading will be accomplished by the temporary use of planned parking spaces, or in parking spaces limiting a vehicle's permitted parking time (e.g., parking stalls designated with 20 minute parking limits).
- C. Additions or enlargements of structures, modification of floor areas dedicated to living and working spaces, or any subsequent change in the approved live/work units shall require a specific plan amendment, administrative clearance or conditional use permit, depending on the nature and scope of change being sought.
- D. Live/work units and buildings must comply with any requirements imposed by the Building, Fire, Community Development, Police, and Public Works Departments intended to protect the public health, safety and welfare.
- E. An administrative approval or conditional approval of the commercial/work component of the live/work units shall be granted to the owner of the unit. Approvals of commercial uses may not be transferred between units. A copy of all conditions of the approval of the project shall be provided to all future owners/occupants of the building prior to their execution of a lease or purchase agreement for the live/work unit. Project conditions are required to be recorded with the County Recorder's Office prior to exercise of entitlement.
- F. Businesses using commercial vehicles are prohibited.

5.1.4 Increased Building Height Standards

The Artesia Corridor Specific Plan seeks to apply innovative planning and design solutions to create a sense of place at multiple scales. Allowing buildings to increase in height can generate more efficient uses of land, increase open space areas, and create landmark buildings. The area south of main street offers the greatest potential to construct taller residential and mixed-use buildings. Accordingly, the Specific Plan incorporates standards to allow buildings located south of main street in Planning Areas 2, 3, and 4 to achieve a building height up to a maximum of 65 feet (75 feet with projections), provided additional amenities and features are incorporated. Figure 5.1d provides an illustration of the maximum general height standards and the maximum height permitted south of Main Street.

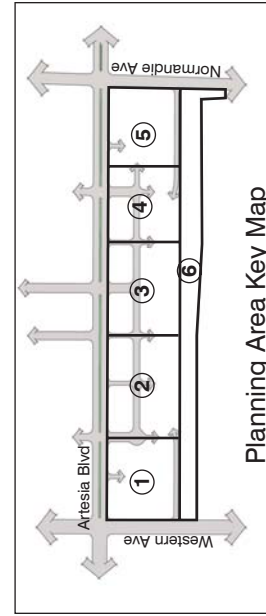
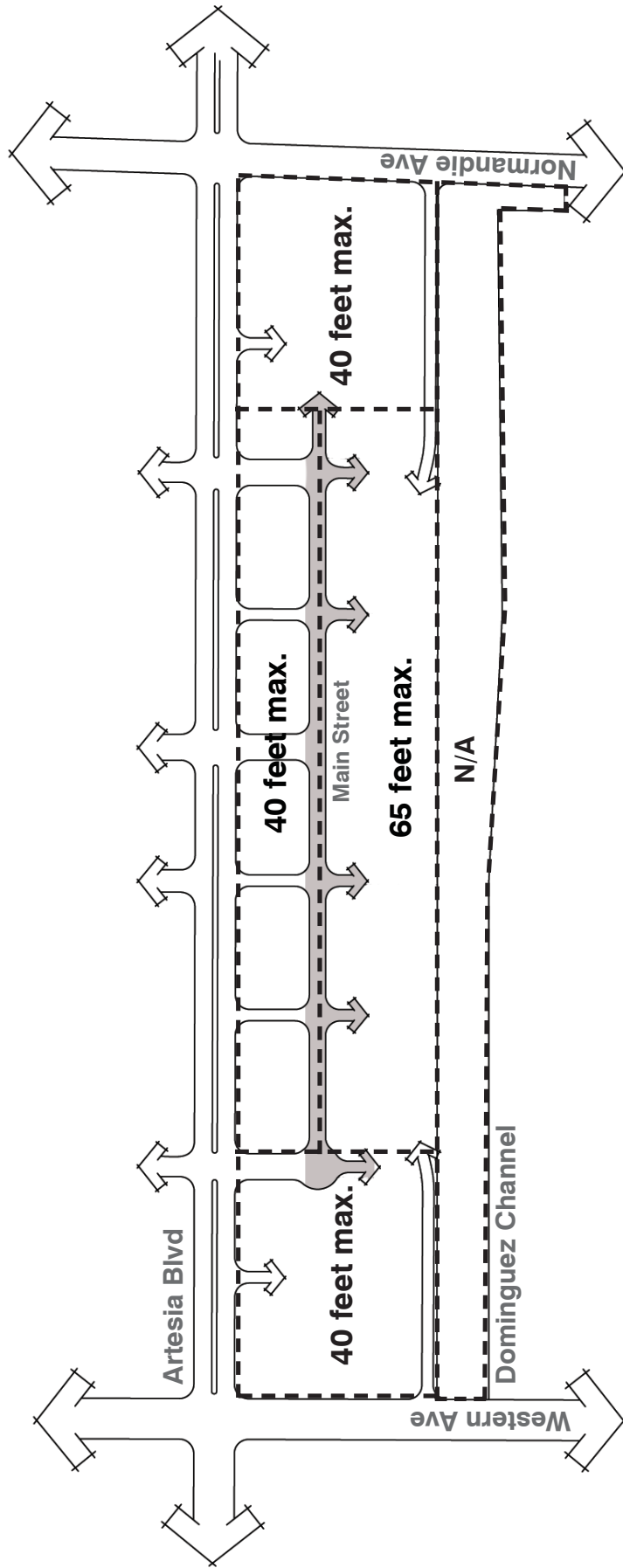
These features and amenities shall be granted credit in the form of points. To achieve a base building height of 55 feet, a project must achieve 125 points. To achieve a base building height of 65 feet, a project must achieve 225 points. These features can serve to satisfy multiple requirements, such as common open space, public plaza space, increased building height, and public art requirements.



| Table 5-1c Features to Obtain Increased Building Height | | |
|--|--|------------------------------------|
| FEATURE | Maximum Points Per Feature | Maximum Points Per Building |
| Semi-subterranean or below grade parking | 75 | 75 |
| Mixed-use building with residential above retail | 75 | 75 |
| Large common open space or public plaza area along Main Street (minimum 40 feet x 50 feet) | 50 | 50 |
| Large common open space or public plaza area (minimum 50 feet x 50 feet) | 25 | 50 |
| Pool and spa | 25 | 25 |
| Landscape terraces visible from the public realm | 15 | 15 |
| BBQ facility with multiple grills and picnic benches | 15 | 15 |
| Formal garden space (minimum 20 feet x 20 feet) (e.g., rock garden, japanese garden) | 10 | 20 |
| Large water feature (1) | 10 | 20 |
| Large piece of artwork/sculpture (2) | 10 | 20 |
| Community multi-purpose room or clubhouse | 10 | 10 |
| Small water feature (1) | 5 | 15 |
| Small piece of artwork/sculpture (2) | 5 | 15 |
| Outdoor fireplace | 5 | 5 |
| Other amenities as approved by the Community Development Director | To be determined by the Community Development Director | |

NOTES FOR TABLE 5-1c:

1. Water features are focal points or decorative elements that contain still or moving water. Water features can be at the ground level (e.g., pop-jets) or elevated (e.g., fountains). Large water features are at least 100 square feet in area if flat and at ground level, or at least ten feet wide or five feet tall if elevated. Small water features are up to 100 square feet in area if flat and at ground level, or up to ten feet wide or five feet tall if elevated.
2. Large pieces of artwork or sculpture are at least 100 square feet in area if flat (e.g., mural or mosaic), or at least ten feet wide or five feet tall if elevated. Small pieces of artwork or sculpture are up to 100 square feet in area if flat and at ground level, or up to ten feet wide or five feet tall if elevated.



Maximum Building Height

Building Height Area

Main Street (Conceptual Alignment)

note: height figures refer to base building heights

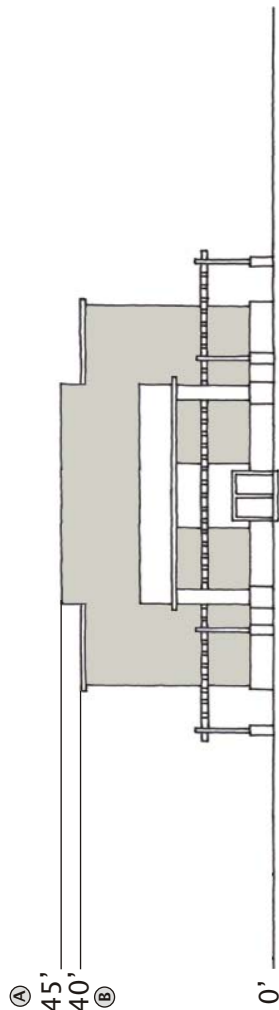
source: The Planning Center, 2006

Figure 5.1d Maximum Building Height by Location



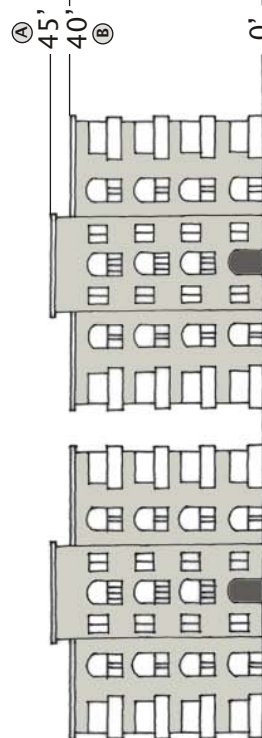
GENERAL STANDARD MAXIMUM HEIGHT

Commercial

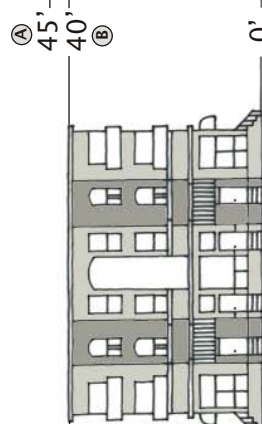


- Ⓐ 45' high maximum with architectural projections
 - Ⓑ 40' high maximum on base building
- note: height measured from finished grade*

Residential



Live/Work or Residential



Mixed-Use



- Ⓐ 45' high maximum with architectural projections
 - Ⓑ 40' high maximum on base building
- note: height measured from finished grade*

source: The Planning Center, 2006

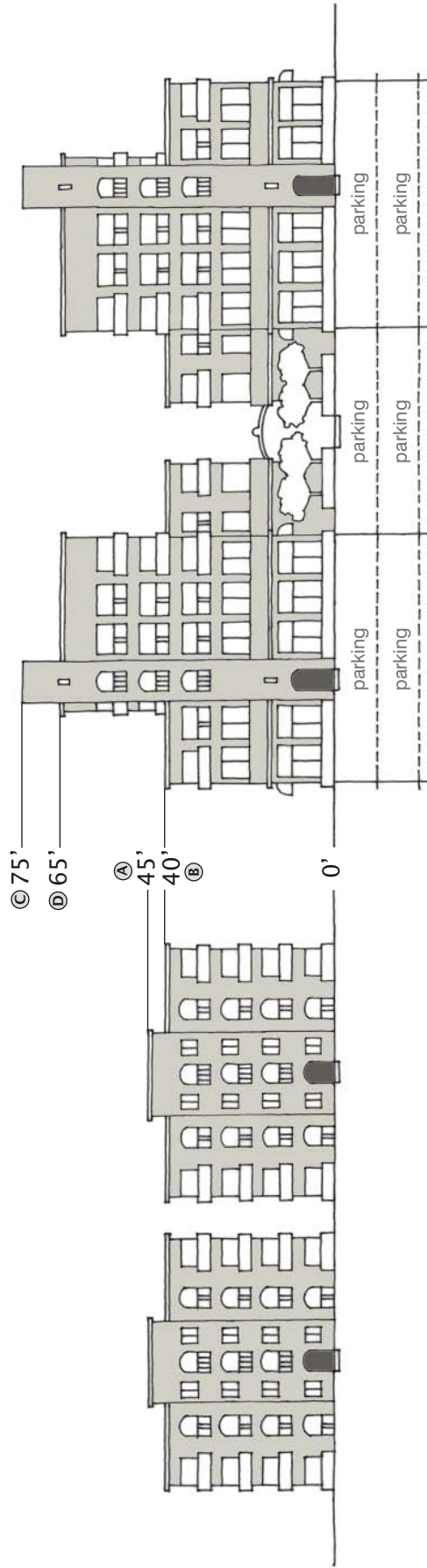
Figure 5.1e Illustration of Building Height Standards



ARTESIA CORRIDOR SPECIFIC PLAN

INCREASED HEIGHT ALLOWANCE FOR BUILDINGS SOUTH OF MAIN STREET*

GENERAL STANDARD MAXIMUM HEIGHT



- Ⓐ 45' high maximum with architectural projections
- Ⓑ 40' high maximum on base building
note: height measured from finished grade

- Ⓒ 75' high maximum with architectural projections
- Ⓓ 65' high maximum on base building
note: height measured from finished grade


*Must include additional features per Table 5-1 c to achieve heights greater than the general standard.



5.2 RECREATION AND OPEN SPACE STANDARDS

The mixed-use character of the Artesia Corridor Specific Plan requires special recreation and open space standards to ensure adequate open space for the residents and consumers while also recognizing the unique types of spaces that work best within a mixed-use setting. Residents living in mixed-use developments tend to consist of singles or young couples, who do not require the large private and common open spaces normally associated with families and traditional suburban development. Instead, smaller, centrally located plazas and recreation areas are encouraged to provide a more intimate, urban lifestyle. Special provisions are incorporated that promote the placement of plazas along the Main Street to increase social activity, enhance walkability, and provide open space connections throughout the site.

5.2.1 Residential Standards

- A. A minimum of 150 square feet of outdoor usable common and private open space shall be provided per dwelling unit.
 1. A minimum of 70 square feet per unit shall be private with a minimum dimension of seven feet in any direction. Private open space shall be accessible directly from the living area of the unit, in the form of a fenced yard, patio, deck or balcony.
 
 2. A minimum of 50 square feet per unit shall be usable common open space.
 - a. Usable common open space shall have a minimum level surface dimension of 20 feet in any direction and a minimum area of 400 square feet.
 - b. The dimensions for usable common open space areas shall be measured from the outside of any private open space attached to a unit at ground level.
 - c. The dimensions for usable common open space may include the building setback area if the buildings facing the open space area display a high degree of articulation and the building setback area is heavily landscaped. Landscaping may consist of in-ground or above ground plantings (potted plants).
 - d. Usable common open space shall not include any of the following:
 - i. Any area counted as private open space
 - ii. Sidewalks and paved pathways
 - iii. Any portion of open and enclosed parking areas, garages, streets, driveways, automobile turning aisles or turnaround areas
 - iv. Storage areas (refuse or otherwise) or any area fenced or otherwise inaccessible to the residents
 - v. Slope areas exceeding 5 percent
 - vi. Areas within public rights-of-way along Artesia Boulevard, Western Avenue, and Normandie Avenue



3. A minimum of 30 square feet per unit shall be provided in the form of common or private open space. This space can also be referred to as “flex space” and can be added to the private or common open space areas.

5.2.2 Live/Work and Mixed-Use Residential Standards

- A. A minimum of 100 square feet of usable common open space shall be provided for each live/work unit or any residential unit in a mixed-use building.
 1. Usable common open space shall have a minimum level surface dimension of 20 feet in any direction and a minimum area of 400 square feet.
 2. The dimensions for usable common open space areas shall be measured from the outside of any private open space attached to a unit at ground level.
 3. The dimensions for usable common open space may include the building setback area if the buildings facing the open space area display a high degree of articulation and the building setback area is heavily landscaped. Landscaping may consist of in-ground or above ground plantings (potted plants).
 4. Usable common open space shall not include any of the following:
 - a. Any area counted as private open space
 - b. Sidewalks and paved pathways
 - c. Any portion of open and enclosed parking areas, garages, streets, driveways, automobile turning aisles or turnaround areas
 - d. Storage areas (refuse or otherwise) or any area fenced or otherwise inaccessible to the residents
 - e. Slope areas exceeding 5 percent
 - f. Areas within public rights-of-way along Artesia Boulevard, Western Avenue, and Normandie Avenue



- B. The residential open space requirements of mixed-use buildings must be met in addition to any public plaza space requirements generated by nonresidential portions of the mixed-use building.

5.2.3 Commercial Standards

- A. Each development with a commercial gross leasable area (GLA) of 25,000 square feet or less shall provide or contribute toward public plaza space equal to 10 percent of the total GLA of the building. Each development with a commercial GLA greater than 25,000 square feet shall provide or contribute towards public plaza space equal to 5 percent of the total GLA of building.
- B. Public plazas shall have a minimum dimension 20 feet in any direction and a minimum area of 400 square feet.
- C. Public plazas may consist of pedestrian-accessible spaces, including outdoor seating areas, open space, water features and landscape areas.



- D. Outdoor eating areas provided as part of private eating establishments cannot be counted towards the public plaza requirement, unless the eating areas are open and accessible to the public, with no fencing or other barriers or obstructions. If the eating areas are enclosed by fencing or landscaping no greater than four feet in height, these areas may count toward up to 20 percent of the total public plaza requirement.
- E. Public plazas shall exclude parking areas, roadways and the first five feet surrounding all sides of the buildings.
- F. Development existing prior to the adoption of the Specific Plan is exempt from this requirement. This requirement shall apply to new square footage constructed. However, if new commercial square footage is added to an existing building, the requirement shall be enforced for the new square footage as follows:
1. The applicant shall only be required to provide plaza space as 5 percent of the new additional square footage of GLA. For example, if an existing 40,000 square foot commercial building adds 10,000 square feet of GLA, 500 square feet of public plaza space must be provided (10,000 x 5%).
 2. This requirement shall be waived if existing public plaza space provided by the existing building is of sufficient size to satisfy the total public plaza requirement of the existing and added square footage. For example, if an existing 40,000 square foot commercial building adds 10,000 square feet of GLA, it would be considered a 50,000 square foot building. If the existing building already provides a 2,500 square foot public plaza (50,000 x 5% = 2,500 square feet), then no additional public plaza space shall be required.
- G. Commercial or office space provided within a live/work unit is exempt from the public plaza requirement.



5.2.4 Special Provisions

- A. Individual uses do not need to satisfy their usable common open space or public plaza space requirement immediately next to their buildings, and are instead encouraged to coordinate with other uses to provide larger open spaces that are centrally located along the main street and serve multiple buildings.
1. The usable common open space requirements of residential, live/work, and mixed-use residential units can be combined into one or more large spaces to satisfy the usable common open space requirements of all residential uses, so long as the space is located along or is directly accessed and visible from main street.
 - a. For example, 50 residential units and 20 live/work units would respectively generate a usable common open space requirement of at least 2,500 square feet and 2,000 square feet. A 4,500 square foot plaza, which is located along main street and is accessible to both uses, can be constructed to satisfy the total usable common open space requirements of both uses.



- b. Alternatively, for the example cited above, a 2,500 square foot plaza that is adjacent to the live/work uses along main street and is accessible to both uses, can be constructed to satisfy the 2,000 square foot usable common open space requirement of the live/work units and 500 square feet of the residential units' usable common open space requirement.
 2. The usable common open space and public plaza requirements of mixed-use buildings that contain residential and commercial uses can be combined into one or more large spaces to satisfy the usable common open space requirements of all residential uses, so long as the space is located along or is directly accessed and visible from main street.
 3. Usable common open space or public plaza requirements of a proposed project may be added to an existing open space or public plaza area, so long as the proposed open space or public plaza area is contiguous to the existing open space or public plaza area and permission is granted by the existing property owner.
- B. A maximum of 33 percent of the area used to satisfy the usable common open space or public plaza requirements can be landscaped with vegetation or hardscape materials that cannot be walked or sat upon. In-ground flowerbeds are an example of vegetation that cannot be walked or sat upon.
- C. Development applications shall be accompanied by design plans for the private open space, common open space and public plaza areas, specifying location and extent of landscaping, irrigation systems, structures and circulation (vehicular, pedestrian and bicycle).

5.2.5 Public Art

Public art is an instrumental feature that can be used to create a connection between the public and any particular project or space. Public art makes spaces more interesting, helps to distinguish one place from another by creating landmarks that are easily recognizable, and creates a unique shopping, working or living environment. Art also revitalizes public spaces and makes them more welcoming. By enhancing the overall quality of a project and giving it a unique character, public art increases a project's value. The following are public art requirements and guidelines.

- A. Public art within the Specific Plan project area shall be provided for the following:
 1. New residential, commercial, live/work, or mixed-use development having total project costs of \$300,000 or more, as determined by the City's valuation of building permits issued for the development.



2. Expansion of existing buildings or remodeling of existing buildings when any such work has a building permit valuation of \$300,000 or more.

B. Public art provided shall have a value equal to one-half of 1 percent (0.50%) of the total building costs (as measured by building permit valuations), excluding land, site development, off-site requirements and remediation costs. The value of the public art shall include the art piece itself and the cost of installation.



- C. Public art may be installed concurrently with and adjacent to each building that triggers the public art requirement; or the value of the required public art for each building can be consolidated, or banked, and applied to the provision of larger installations that serve multiple buildings.
- D. The public art requirements shall not apply to reconstruction of structures that have been damaged by fire, flood, wind, earthquake or other calamity.

5.2.6 Parks

The City parks and open space requirement for residential development of three acres of park per 1,000 residents can be met through a combination of land dedication, improvements, and in-lieu fees per Chapter 17.20 of the Gardena Municipal Code (2006). The intent is to provide an appropriate amount and distribution of public and/or private open spaces through a combination of open spaces in and near the project.

5.3 PARKING STANDARDS

Parking is an important issue in the Artesia Corridor Specific Plan. The project area is surrounded by arterial streets on the northern and western sides, the railway on the eastern side, and the Dominguez Channel on the southern side, leaving little opportunity for on-street parking. As a result, the project must be able to park all users within the boundaries of the Specific Plan.

However, this does not mean that the site should be dominated by parking lots. Due to the potential mix of uses on the site, shared parking is encouraged. Shared parking can reduce the amount of land needed for parking, creating opportunities for more compact development, more space for pedestrian circulation, or more open space and landscaping. Shared parking is allowed subsequent to a shared parking analysis approved by the Community Development Director. As part of the study, the applicant will have to demonstrate that the proposed uses have differing peak-hours of parking demand or that the total parking demand at any one time will be adequately served by the total number of parking spaces proposed. Special provisions concerning shared parking can be found in the Section 8.3 of this Specific Plan.





**Table 5-3
Parking Standards**

| TOPIC | STANDARD | NOTES |
|-------------------------------------|---|---|
| COMMERCIAL | | |
| General Commercial | 1 per 250 SF GLA | |
| Restaurant | 1 per 100 SF GLA | Eating establishments attached to other uses and containing a seating area of 1,250 SF or less shall be treated as general commercial. |
| RESIDENTIAL OR LIVE/WORK | | |
| Resident Spaces | 2 spaces per unit | Both spaces must be enclosed within a garage. Studio units smaller than 800 square feet must only provide 1.5 parking spaces enclosed within a garage and 0.5 uncovered parking spaces per unit. |
| Guest Spaces | 0.5 space per unit | Spaces may be uncovered. |
| MIXED-USE | | |
| Residential - Resident | 2 spaces per unit | |
| Residential - Guest | 0.5 space per unit | |
| General Commercial | 1 per 333 SF GLA | |
| Restaurant | 1 per 100 SF GLA | Eating establishments attached to other uses and containing a seating area of 1,250 SF or less shall be treated as general commercial. |
| STRUCTURED PARKING | | |
| Maximum Height | 40/45 feet | The maximum base building height is 40 feet. With architectural projections, the building may reach 45 feet. Increased building height standards (see Section 5.1.4) may be applied if the structured parking facilities are constructed as part of a residential, live/work, or mixed-use building. |
| Setbacks | see Table 5-1b | Parking structures shall be considered a commercial use >20,000 square feet when determining setbacks. |
| Landscaping | 50% coverage of visible concrete surfaces | Parking structure facades shall achieve 50% coverage of visible concrete surfaces with landscaping. Coverage can be achieved through measures such as planters along the visible edge of the structure planted with cascading vines, or through a vertical trellis surface with vines planted at each parking level, or by other means. |
| TANDEM PARKING | | |
| Residential | up to 50% | Tandem parking permitted for up to 50% of residential units. |
| Live/Work or Mixed-Use | up to 25% | Tandem parking is permitted for up to 25% of live/work units or residential units in a mixed-use building. |
| General Standards | | <p>Parking for both tandem spaces must be subterranean, structured, tuck-under, or garaged and both spaces must be assigned to the same designated unit.</p> <p>Each unit using tandem parking shall have at least 200 cubic feet of enclosed, weather-proofed and lockable storage space for the sole use of the unit owner, with a minimum horizontal interior dimension of 3.5 feet.</p> |
| PARKING DIMENSIONS | | |
| Compact Spaces | | |
| head-in/diagonal | 8 feet x 16 feet | |
| ratio of compact to standard spaces | 25% | Compact spaces are only permitted in structured or subterranean parking. |
| Standard Spaces | | |
| head-in/diagonal | 9 feet x 18 feet | |
| parallel | 8 feet x 25 feet | |



| Table 5-3 Parking Standards | | |
|--|-------------------------------------|--|
| TOPIC | STANDARD | NOTES |
| Tandem Spaces | | |
| single-family attached | 12 feet x 36 feet | In single-family attached products, the garage dimensions of tuck-under or garaged tandem parking must be no smaller than 12 feet wide by 36 feet long, as measured from the interior walls. |
| live/work (tuck-under/garaged) | 12 feet x 40 feet | In live/work products, the garage dimensions of tuck-under or garaged tandem parking must be no smaller than 12 feet wide by 40 feet long, as measured from the interior walls. |
| structured or subterranean | 9 feet x 36 feet | In structured parking, tandem spaces must be no smaller than 9 feet wide by 36 feet long. |
| BICYCLE PARKING | | |
| Bicycle Parking | 1 rack per 50 nonresidential spaces | <p>For any development that requires 50-100 nonresidential parking spaces, 1 rack that can support 2 bicycles shall be provided.</p> <p>For any development that requires more than 100 nonresidential parking spaces, 1 rack that can support 4 bicycles or 2 racks that can support 2 bicycles shall be provided.</p> <p>This requirement does not apply to live/work units.</p> |

5.4 NOISE AND VIBRATION STANDARDS

5.4.1 Noise

- A. Where residential uses are potentially exposed to interior or exterior noise levels greater than those permitted by the Gardena Municipal Code (2006), certification from a licensed acoustical engineer shall be obtained to document attenuation to those maximum levels. The exterior standards shall be measured either at the closer of the property line or the nearest noise sensitive use such as a patio, yard or landscaped open space.
- B. Commercial uses shall be designed and operated, and hours of operation limited, where appropriate, so that neighboring residents are not exposed to offensive noise, especially from traffic, trash collection, routine deliveries or late-night activity. No use shall produce continual loading or unloading of heavy trucks at the site between the hours of 8 p.m. and 7 a.m.
- C. At the time that Master Site Plan review is conducted for the development of the Planning Areas of the Specific Plan, the Applicant shall prepare and submit to the City a site-specific noise study and shall design and implement noise attenuation measures such as walls, berms, landscape buffers, soundproofing or other features that would reduce exposure to excess roadway noise.
- D. Prior to issuance of building permits, the applicant shall submit a detailed acoustical study demonstrating that all project structures will meet applicable City interior noise levels and exterior living area noise levels, in accordance with applicable noise standards and zoning regulations.
 1. The study shall be prepared by a City-approved acoustical expert, to the satisfaction of the Community Development Director.



2. The study shall document projected ultimate noise exposure for interior office, retail and residential space and shall demonstrate that Project design plans have incorporated adequate sound attenuation measures to achieve the applicable noise standards.
- E. Noise mitigation and proper design may include, but shall not be limited to, building orientation, double or extra-strength windows, wall and ceiling insulation, and orientation and insulation of vents. Where it is necessary that windows be closed in order to achieve the required level, means shall be provided for ventilation/cooling to provide a habitable environment.

5.4.2 Vibration

- A. No use, activity or process shall produce continual vibrations or noxious odors that are perceptible without instruments by the average person at the property lines of the site or within the interior of residential units on the site.

5.5 ENERGY CONSERVATION

The California Energy Code, Part 6 of Title 24, has codified many ways to reduce energy usage. It addresses lighting, building construction and heating/cooling systems. Compliance with this Code results in a reduction of energy usage for any given building or complex. Additional steps can be taken to further reduce the energy usage and reduce operating costs of a building or complex. The Artesia Corridor Specific Plan will meet or exceed the requirements of Title 24 through measures that may include:

- A. Use of light-colored roofing materials to reflect heat and reduce cooling requirements in residential and retail buildings. Energy Star-labeled roofing materials are encouraged.
- B. Installation of Energy Star-labeled appliances (e.g., water heaters) to the greatest feasible extent. Solar, electric (efficiency rating of at least 0.92) or lower-nitrogen oxides (as defined by the Air Quality Management District) gas-fired water heaters are strongly encouraged.
- C. Participation in programs offered by or sponsored by local utilities such as:
 1. California Energy Star New Homes Program
 2. Residential Property Development Program
 3. California Home Energy Efficiency Rating System (CHEERS) Program
 4. Savings by Design Program
- D. Development of a recycling program for residential and commercial uses to recycle paper, glass, plastic and other by-products of business or residential activities.



6. DESIGN STANDARDS AND GUIDELINES

Quality development is achieved through attention to detail that is implemented from the initial conception of a project to the final construction of buildings, pathways, entry features, signage, and other design elements. This section establishes design standards and guidelines to ensure that Artesia Corridor will possess an identifiable look and feel, and to establish an expectation for quality development features in the specific plan through site planning, architecture, landscape, and streetscape components.

All development within the Artesia Corridor Specific Plan must address the fundamental elements of the design features outlined in this section. These standards and guidelines comprise the basic framework that the City will use to evaluate proposed development projects. The pictures presented in this section are intended to convey the general design purpose of the guidelines, and are not intended to require the specific design style depicted.

Like development standards, design standards constitute regulations, requirements and by-laws by which development must abide, and are indicated by the use of the word “shall.” Design guidelines generally use the word “should” and identify actions or outcomes that are encouraged but not mandatory.

6.1 SITE DESIGN

The integration of buildings, entries, parking lot layout, opens spaces, and pedestrian and vehicular circulation is critical to achieving an overall sense of place. With the mix of proposed land uses within the Artesia Corridor area, it is important to set guidelines to coordinate site planning between distinct building types and to ensure the connectivity of public spaces. The guidelines below are organized by the different components of site design.

6.1.1 Building Orientation and Site Planning

- A. Buildings and major pedestrian entrances should be oriented towards centers of activity, such as the internal main street, pedestrian pathways, and public plazas.
- B. Where possible, buildings should be clustered along the length of the internal main street to demonstrate a positive functional relationship to one another. The clustering of buildings should provide focal areas and define public spaces. Where clustering is impractical, a visual link between structures should be established through the use of an arcade, trellis, or other open structure.
- C. Provide special, enhanced, “signature” architecture at prominent locations. Buildings with unique architectural elements, such as clock towers, and landmark structures should be positioned on corners of significant intersections or entryways to enhance the sense of arrival and project monumentation.
- D. Buildings at corners should receive special treatment to enhance the pedestrian experience, such as building cut-offs and corner entrances with additional architectural details.
- E. Commercial buildings shall provide for loading and service areas to be located away from public view and screened from residential buildings.



Unique architectural elements should be placed on corners of significant intersections.



Pedestrian access should link residential areas to the internal main street.



Public spaces located along the internal main street maximizes their use.



Straight-in, diagonal, or parallel parking should be provided along the main street.

- F. Commercial buildings should face pedestrian streets and plazas and should incorporate design features that provide visual interest at the street level.
- G. Residential projects shall include pedestrian access towards the main street and public spaces to link these areas to the rest of the project.
- H. Residential structures should be configured and oriented to afford a sense of individuality and privacy and to create small scale public spaces.
- I. Residential building orientation and placement of windows and balconies should maximize view potential and shield residents from commercial loading areas, utilities, and mechanical equipment, trash areas, and major street noise.

6.1.2 Pedestrian Pathways and Public Open Spaces

- A. Uses should coordinate their open space requirements to provide larger public spaces that are centrally located and serve multiple uses.
- B. Public spaces should be located along the internal main street, near building entrances or areas of high pedestrian traffic, to ensure their use and highest functionality.
- C. Public spaces shall be oriented to maximize their visual and physical link from adjacent streets and pathways.
- D. Pedestrian pathways should emphasize links between different uses and provide connectivity through the project.
- E. Pedestrian connections through parking areas should provide landscaping and amenities to create visual interest, pedestrian access, and rest breaks over long distances of pavement.
- F. Safety and visibility shall be considered in the design of both pathways and public spaces.

6.1.3 Parking and Parking Structures

- A. Parking areas shall be located behind or to the side of commercial buildings where possible.



- B. Parking areas should be clustered where feasible, and large, expansive parking lots should be minimized to the greatest extent possible. This maximizes security and efficient access.
- C. On-street parking should be provided along the main street, and should include areas for straight-in, diagonal, or parallel parking, particularly near high activity areas.
- D. Where parking structures are needed, shops, offices, or other commercial spaces should be incorporated on the ground level of the parking structure along street frontages, where feasible to maintain the pedestrian experience.
- E. Entrance and exit points should be well marked with streetscape and landscape features.
- F. Public parking for commercial or office uses should be separated from private residential parking areas.
- G. Residential parking areas should be located as close as possible to residential uses.



Utility areas should be discretely located and screened from view.

6.1.4 Service, Trash, and Utility Areas

- A. Service and utility areas should be incorporated within the building. If not possible, orient these areas away from public view and provide ornamental screening.
- B. Exterior on-site facilities, including sewer, gas, water, electric, telephone, and communications equipment should be installed underground where feasible. Transformers and other utility equipment that must be above ground should be screened and incorporated into the landscape wherever possible.
- C. Trash enclosures shall be enclosed within the building envelope. If infeasible, locate them in discrete places, and design enclosures to architecturally integrate with the overall design theme of the development. Trash enclosures should be planted with vines if located adjacent to or within a landscaped area to help screen the enclosure.



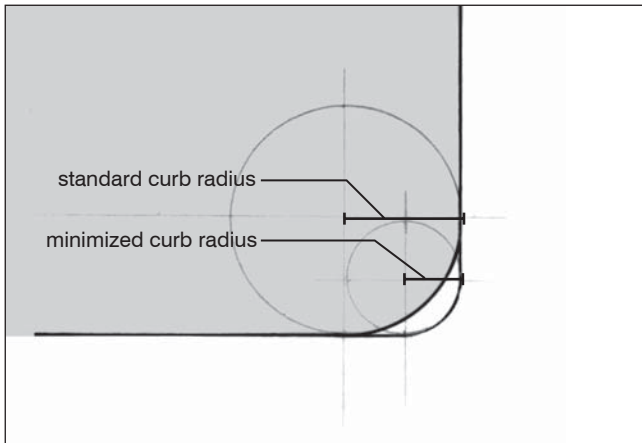
Service areas and trash enclosures should be incorporated into the building structure and screened from view.



Bulb-outs at intersections, and special paving treatments for crosswalks act as traffic calming elements.

6.1.5 Traffic Calming

- A. Use of neck-downs, bulb-outs (which can also act as planters), and other traffic calming



Vehicles must slow to turn when the radius of the corner is minimized.



Use of trellis and landscaping provides visual relief to break up building facades.



Building articulation and modulation provides an attractive streetscape.

measures are encouraged along the length of the internal main street.

- B. Minimize the radius of corners where pedestrian walkways cross at intersections.
- C. Utilize a variety of special paving treatments to identify and visually enhance intersections and pedestrian crossings.
- D. Discourage cut-through vehicular movement to ensure that residential areas are preserved as “slow-go” streets.

6.2 ARCHITECTURE

The purpose of the architectural standards and guidelines is to ensure a high level of architectural quality with attention to details. The Artesia Corridor Specific Plan does not recommend a particular architecture style for development, but recommends consistency in architectural style throughout the project, and promotes a unique style of architecture achieved through the creative use of massing, roof forms, and facades. Architectural styles complementary of the proposed commercial development should be applied and integrated into any proposed residential components of the project.

6.2.1 Massing, Scale, and Form

- A. Buildings should be divided in distinct massing elements, and facades should be articulated with significant use of architectural elements and details.
- B. Building facades should be detailed in a way that makes them appear smaller in scale. This can be achieved by articulating the separate floor levels with horizontal bands or by increasing the level of detail on the building at the street or ground level.
- C. Avoid repetitive elevations by using a variety of building forms.
- D. Allow for visual relieve to break up long, blank, inarticulated building facades. There shall be no long flat expanses of walls that exceed 50' (100' for buildings larger than 50,000 square feet in size) without at least two of the following: color change, material change, texture change, plane projections or recesses, trellises or vines, balconies, or windows.



- E. Maximum total bland wall (without windows or entrances) shall not exceed 30 percent of the first story wall.
- F. Vertical and horizontal articulation should be incorporated which include the strategic placement of window and door openings, or through the use of balconies, awnings, and canopies.
- G. For commercial development, public plazas, outdoor dining, and other pedestrian oriented activities may divide ground-level building facades.
- H. For residential development, visual interest and distinction between units is encouraged with projecting features such as balconies, porches, bays, and dormer windows.

6.2.2 Style

- A. The overall character of the Artesia Corridor project should be established through a chosen set of complementary architectural styles that are compatible with the surrounding architecture of the existing community.
- B. Chosen architectural styles should incorporate high quality architectural detail that includes the use of arches, arcades, loggias, towers, variations in building form, and color blocking to define buildings.
- C. Visual diversity should be promoted through the use of different architectural styles, but chosen architectural styles must be complementary. For example, different architectural styles may be used to distinguish between residential and commercial buildings, but the styles must be harmonious.



Architectural details such as awnings and planters add interest to the building facade.

6.2.3 Design Details

- A. Design details should be included for each building. These include some combination of the following:
 - Light fixtures
 - Balconies on the second or third floors
 - Decorative bulkheads
 - Shutters
 - Awnings for windows or balconies
 - Decorative cornice
 - Pedestrian arcades
- B. Building elements should be designed in a way that enhances the visibility of merchandise and encourages window shopping.
- C. Depth and shadow interest should be incorporated with the addition of offsets, pop-outs, and overhangs.
- D. Energy efficiency is encouraged, through the use of window overhangs, arcades or loggia, solar hot water heating, advanced heating and cooling systems, or other conservation measures.



Example of different colors, textures, and materials.



6.2.4 Material & Color

- A. High quality materials that weather well over time, with the appropriate use of colors and textures, should be used for building materials and design elements.
- B. Materials and colors should be consistent with the architectural style.
- C. Colors should include a base color and accent colors. Generally, a minimum of three colors should be used for each building. Color schemes should be selected with a harmonious range of accent materials and roof profile and colors.
- D. To avoid monotony, a variety of color schemes is encouraged to provide visual interest. However, building color and materials should be complementary throughout the project.
- E. Prohibited materials include: plain concrete block, plain concrete, corrugated metal, plywood, sheet pressboard or vinyl siding.

6.2.5 Roof Form

- A. Breaks should be provided in roofline ridges so that there is variety in roof form and elevation appearance.
- B. A variety of roof types are permitted and encouraged within the project, including hip, gable, and shed roofs.
- C. The roofs should be consistent with the architectural concept and the style, materials, and scale of the building.
- D. Roof heights, pitch, and planes should vary to create interplay between the roof and walls of the structure. Varying roof pitches on the same building shall be avoided unless they are integral to the architectural style or extending over porches and balconies.



Tower features add a landmark character to the project.

- E. Roofing trim materials should be of similar materials and complementary colors. Roof vents and appurtenances shall be painted to match the roof color.
- F. Roof overhangs are permitted and shall be designed as an integral part of the roof form.

6.2.6 Entryways

- A. Building entryways shall be highlighted to establish an architectural focal point. Roof overhangs, canopies, columns, lighting features, landscaping treatments, and signs are examples of architectural elements that can achieve this.
- B. Entry doors should be recessed to allow the pedestrian space to transition from the sidewalk to the interior of the building.



6.2.7 Tower Features

- A. Tower features help to create the image of a landmark place and add architectural interest to otherwise standard structures.
- B. The scale of tower elements should be designed to create an architectural statement, but should not be so grand as to appear imposing to plazas and walkways adjacent to them.

6.2.8 360-degree Architecture

- A. 360-degree architecture shall be used to ensure that areas of structures typically considered as the “rear” of the building still convey the quality and attention to detail of the project.
- B. Elevations which do not face major streets should not be neglected. Each side of the building should have similar design features.
- C. Vines and trellises are encouraged for large expanses of building faces to cool the building surface and soften the wall’s appearance.

6.3 STREETScape

Streetscape incorporates community features and elements related to the street environment such as signage, lighting, furnishings, walls & fences, paving materials, and public art. These components also apply to public spaces and pathways. Design and use of streetscape elements work to create pedestrian friendly areas along the internal streets and public spaces.

The different streetscape components identified below should carry common design themes through architecture, color palettes, and use of quality materials. With consistent and complementary design themes across the different streetscape elements, the Artesia Corridor project presents a unified image and an established a sense of place.

6.3.1 Signage

Signage works to identify places, provide direction, and advertise businesses. Along with communicating information, signage should add to the character of the community and reinforce a sense of place. The systems of signage addressed in these guidelines are monument entry signs, directional signs, retail monument signs, and building identification signs.

In addition to these guidelines, a Sign Program shall be required on a project by project basis. The Sign Program will identify the hierarchy of signs with a common theme, as well as specifying the signage location and style. These Sign Programs shall be consistent and complementary within the specific plan.

General guidelines that pertain to all of signage within the Artesia Corridor project are listed below, followed by guidelines specific to the different signage systems.



Signs should be externally illuminated.



A. General Guidelines:

1. Signs shall consist of high quality materials and color palettes that reflect the architectural themes of the Artesia Corridor project. The designs should also complement the architecture of the surrounding environment.
2. Internally illuminated signs are discouraged. Externally illuminated signs, or the backlighting of individual sign letters should be the standard.
3. Signage shall be well maintained, especially display surfaces.
4. Location and placement of signs should not obstruct pedestrian or vehicular movement.
5. Pole signs shall not be used.

B. Monument Gateway Signs:

Major intersections, or gateways, present a first impression of the Specific Plan area and are intended to be distinct and identifiable. Monument gateway signs should be used to indicate entrance to the City of Gardena and the Artesia Corridor project along Artesia Boulevard.

1. Locate monument gateway signs near entries and major intersections for the project.
2. Monument gateway signs require special design treatments to announce arrival, identifying the major development or activity center.
3. Monument gateway signs may flank either side of the entry, or one monument gateway sign may be placed in the roadway median.

C. Directional Signs:

Directional signs are used to identify destination points and direct pedestrians and vehicles to those areas. These signs should not be used for advertising. Directional signs include directory monuments, public service signs, traffic safety signs, and street signs. These signs should embody the character of the Artesia Corridor area, and remain uniform throughout the project.

1. Directional signage should be of consistent design throughout the community.



Directional signage should share consistent design themes throughout the project.

2. Use of a directory monument, to direct users to multiple uses, instead of using several individual signs is preferred.
3. Public service signs should help to direct users to restrooms, telephones, information centers, and other uses. These signs shall be placed at pedestrian eye-level.
4. Street numbers should be clearly displayed on every building. It is important that drivers be able to quickly locate building numbers.

D. Retail Monument Signs:

Retail monument signs, a marker advertising numerous establishments, are used when multiple



retailers are located on one site. This reduces the cluttering of signs that would result if every tenant had its own freestanding sign. These monuments shall be located at entries, pathways, or in landscaped setback areas.

1. Major tenant portion of a monument sign shall not exceed 18 inches in height.
2. There should be elements of consistency between the colors, fonts, and sign dimensions on each of the sign fascias advertising various retailers.
3. As a general rule, no more than three colors should be used on each monument sign.
4. Consistent monument and directional signs will reinforce the community theme.
5. Integration with landscaping is encouraged.



Retail monument signs should have consistent colors and fonts.

E. Building Identification Signs:

Within retail areas, building identification signs indicate the retailer located within each specific tenant space. Signage should be incorporated on building walls, canopy signs, or hanging signs.

1. Signage should be coordinated in design and location along the building facade, and oriented for both pedestrian and vehicular traffic.
2. Signs shall be limited to one per building, per tenant, unless the building fronts an intersection, where two signs are appropriate. Additionally, a second sign is allowed in the form of a hanging sign oriented for pedestrians or under an arcade.
3. Hanging signs are encouraged as an alternative to wall signs, and the use of natural materials are recommended.



Example of building identification signs.

6.3.2 Lighting

Lighting design throughout the Artesia Corridor project will highlight design and landscaping features, reinforce the community theme, and help ensure pedestrian and vehicular safety. Well designed lighting fixtures also establish quality design. The following lighting program addresses lighting for roadways, parking areas, pedestrian areas, architectural and landscape lighting.

A. General Guidelines:

1. Warm white light is encouraged. Blinking, flashing, and oscillating lights are prohibited. Colored lights are not encouraged unless they contribute to the theming of commercial areas or establishments. Overly bright or glaring lights should be avoided.



2. Light fixtures should be consistent throughout the Artesia Corridor project.
3. Fixtures should be complementary to the architectural styles of the area.
4. Standards and fixtures should comply with local and state safety and illumination requirements.
5. Exterior lighting should be designed and located as to not project off-site or onto adjacent uses. This is especially critical with neighboring residential uses.
6. Outdoor lighting associated with the commercial uses shall not adversely impact the on-site or surrounding residential uses, but shall provide sufficient illumination for access and security purposes.
7. Automatic timers should be programmed to maximize personal safety at night while conserving energy. They should be reset seasonally to match the flux of dusk and dawn.



Example of stylized roadway lighting.

B. Roadway Lighting:

Lighting fixtures on roadways contribute greatly to the visual quality of a streetscape, and improve vehicular and pedestrian safety.

1. Stylized roadway lighting fixtures will contribute to the community theme and establish a standard of quality for the entire development.
2. Lighting should be positioned to enhance safety at key points along the roadway, including intersections and crosswalks.

C. Parking Area Lighting:

Lighting for parking areas is crucial to the personal safety of the user, and should be designed at a human scale.

1. Parking area lighting should be designed using many small scaled lights versus using fewer excessively tall lights.
2. Lighting fixtures should be a continuation of the theme of surrounding architectural styles, and be in keeping with the quality of surrounding buildings.

D. Pedestrian Area Lighting:

It is very important that pedestrian areas including public spaces and pathways, are well lit for evening and night use to ensure safety of residents and visitors.

1. Lighting in these areas should be scaled for pedestrians and of a style consistent with the surrounding architectural theme.
2. If the lighting product is a lamppost style, the fixture should be not more than 12 feet high. No lamppost style lighting shall be more than 20 feet high.
3. Where appropriate, “pocket lighting” may be incorporated in walls, stairs, or bollards.



E. Architectural Lighting:

To accent walls, entries, and decorative architectural features, architectural lighting should be included into the design of buildings. Lighting should be used to subtly highlight architectural features, but not as an attempt to advertise the building to passing vehicles.

1. Architectural lighting should either be incorporated into design features of the building or concealed and flush with building walls.
2. Architectural lighting should not be visible in daylight.



Pedestrian spaces should be well lit for evening and night use.

F. Landscape Lighting:

Landscape lighting should be used to highlight important landscape features.

1. String lights, non-blinking with white bulbs, can be used to accent trees or trellises within public spaces to create a festive atmosphere at night.
2. If not designed to add to an area's character, landscape lighting fixtures should be concealed or flush with grade.



String lights may be used to accent trees and create a festive night atmosphere.

6.3.3 Street Furnishings

Street furnishings are decorative and functional elements of the streetscape. Furnishings can include benches, chairs, tables, shade structures, bollards, drinking fountains, tree grates, trash receptacles, planters, bicycle loops, and newspaper stands. Some combination of these should be used as decorative elements for streetscapes and public spaces.

A. General Guidelines:

1. Consistent design themes should be used for all the street furnishing elements throughout the project, and compliment the surrounding architectural styles.
2. Furnishings should be constructed of high quality, durable materials that can withstand the elements without showing wear.



Examples of street furnishings.



Seating areas should be located at around public spaces and at focal points along the streetscape.



Shade structures provide comfort and incentives for pedestrians to use public spaces.



Planters may also be used as bollards.

3. Furnishings should be placed where pedestrian traffic, viewsheds, or building ingress and egress will not be obstructed.
4. Decorative features that prevent skateboarding along street furnishings shall be incorporated within the design of the street furnishings.

B. Seating Areas:

1. Benches should be placed at selected locations along the internal main street, and focused around public spaces.
2. Providing seating areas adjacent to landscaping and public spaces invites people to enjoy the amenities provided.
3. Moveable furniture should be provided to encourage pedestrian use.
4. Planters that also function as seating areas should be provided.

C. Shade Structures:

1. Trellises, umbrellas, gazebos, and other forms of shade structures are encouraged along the internal main street and public spaces within the Artesia Corridor project.
2. Market umbrellas and awnings provide shade, “soften” hard building edges, and add color to the sidewalk area in front of shops and restaurants.

D. Bollards:

1. Bollards should be located at intersections of local internal access streets and external periphery streets and thoroughfares, and in combination with handicapped/wheelchair access ramps, in order to protect pedestrians from vehicular traffic which might inadvertently try to access sidewalk areas via ramp sections along the street edge.
2. Bollards should be placed a maximum of four feet on center in order to prevent vehicular access, and chain loops between bollards



should be avoided in order to minimize “trip and fall” hazards.

3. Bollards should be fabricated and finished to match surrounding street furnishing elements.

E. Tree Grates:

1. Tree grates should be provided for trees along the internal main street.
2. Tree grates should uniform in size according to the tree hierarchy established in the landscape plan. For example, major trees along the main street should share similarly sized tree grates, and different trees clustered around public spaces may share a different set of consistently sized tree grates.



Example of tree grates used in an urban setting.

F. Trash Receptacles:

1. Trash receptacles should be located at all internal street intersections.
2. Additional trash receptacles should be located adjacent to outdoor dining areas and parking areas.

6.3.4 Walls, Fences, and Gates

The design and placement of walls and fences are essential to site design, connectivity, and theme of the project. Gates are also necessary to separate residential and commercial components of the Artesia Corridor project. These features help identify separate areas within the community, as well as provide needed privacy and security.

- A. Although needed in certain locations, solid walls should only be used when absolutely necessary.
- B. Walls, fences, and gates should appear consistent in style and material, complementing the surrounding architectural styles.
- C. Walls, fences, and gates should be constructed of high quality, durable materials.
- D. Landscaping elements should be densely planted and layered to provide screening. Vines and trellises are encouraged to help soften hard edges and screen walls from view.
- E. Where feasible, dense planting and hedges should be used in place of a wall or fence.
- F. Chain link fences shall not be allowed within the Artesia Corridor project.



Dense planting and hedges may be used to screen wall surfaces.



Unique paving treatments highlight public spaces and pedestrian areas along the streetscape.



Paving treatment and public art both add interest to public spaces.



Water features may be interactive elements of the streetscape.

6.3.5 Paving Materials

Distinctive paving treatments give visual clues to users and emphasize different areas within the streetscape and public spaces. Patterned, enriched, or textured paving treatments should be used to denote important crosswalks, highlight pedestrian pathways and public spaces, and delineate parking areas along the internal main street. The use of specialized paving materials is encouraged along the internal main street and through public spaces.

- A. Acceptable paving materials for the streetscape and public spaces include interlocking concrete pavers, native stone, pre-cast pavers, and brick. The use of stained or stamped concrete should be discouraged, unless highly decorative and creative in nature.
- B. Painted paving surfaces should not be used except to indicate traffic lanes or parking spaces.

6.3.6 Water Features and Public Art

Water features and public art are streetscape elements that may be used to highlight public spaces and focal points along the streetscape. These components add an interesting characteristic to public spaces, and may be used as decorative features or interactive elements.

- A. Water features and public art pieces should be located along viewsheds as accent features for public spaces and nodes along the internal main street.
- B. These streetscape elements should not be placed where pedestrian traffic or building ingress and egress would be obstructed.
- C. Water features and public art shall be well maintained to preserve their quality appearance.



7. GENERAL PLAN CONSISTENCY

| Table 7-1 General Plan Consistency Analysis | |
|---|---|
| RELEVANT POLICY | ANALYSIS OF PROJECT CONSISTENCY |
| COMMUNITY DEVELOPMENT ELEMENT—LAND USE PLAN | |
| LU Goal 1: Preserve and protect existing single-family and low/medium-density residential neighborhoods while promoting the development of additional high quality housing types in the City. | |
| <p>LU 1.4: Locate new medium- and high- density residential developments near neighborhood and community shopping centers with commensurate high levels of community services and facilities.</p> <p>LU 1.5: Provide adequate residential amenities such as open space, recreation, off-street parking and pedestrian features in multi-family residential developments.</p> <p>LU 1.6: Ensure residential densities are compatible with available public service and infrastructure systems.</p> <p>LU 1.10: Provide adequate off-street parking, open space and landscaping for both residential and business use in all mixed-use developments.</p> <p>LU 1.12: Require infill development to provide adequate amenities to minimize the impact of such development on the immediate neighborhood and on City services generally, including off-street parking to meet the additional demand placed on street parking.</p> | <p>The Artesia Corridor Specific Plan provides for a mix of up to 300 high-density residential and live/work units and 415,000 square feet of commercial development. Additionally, the project is located across the street from the Gateway Plaza and the Gardena Marketplace.</p> <p>In the Artesia Corridor Specific Plan, the pedestrian spaces are focused along the Main Street within the project, establishing a unique environment for people to gather. The Development Standards section of this Plan identifies a minimum requirement of off-street parking and open space for the residential and mixed-use areas, public plaza space for the commercial areas and prescribes dimensions for walkways and pathways throughout the project.</p> <p>Because the Artesia Corridor Specific Plan will not provide a parkland space directly on the site, in-lieu fees must be paid to pay for up to 2.7 acres of parkland. The Specific Plan recommends that the in-lieu fees required for park requirements be used towards the creation of parkland directly across the Dominguez Channel. The area, identified in Figure 4.6a, is the recommended site for potential parkland and covers approximately 4.3 acres that could be used for passive recreation uses.</p> <p>The project benefits from the ability for proposed utility lines to connect to existing infrastructure facilities. The sewage, water, drainage, and solid waste plans are designed to serve the proposed development within the Artesia Corridor Specific Plan. The project applicants will be required to pay public service impact fees to mitigate impacts on public services.</p> <p>Due to the size and classification of the surrounding roadways, the project must be able to park all users within the boundaries of the Specific Plan. The Development Standards section of this Plan identifies a minimum requirement of off-street parking spaces, including a provision for shared parking to ensure adequate parking is provided.</p> |



Table 7-1
General Plan Consistency Analysis

| RELEVANT POLICY | ANALYSIS OF PROJECT CONSISTENCY |
|--|---|
| LU Goal 2: Develop and preserve high quality commercial centers and clean industrial uses that benefit the City's tax base, create jobs and provide a full range of services to the residents and businesses. | |
| <p>LU 2.1: Require ample landscaping and high level maintenance in all new and existing commercial and industrial developments.</p> <p>LU 2.2: Encourage the assembly of smaller commercial properties into larger centers and discourage the subdivision of larger commercial/industrial sites into smaller parcels.</p> <p>LU 2.4: Provide neighborhood commercial centers with convenient and safe pedestrian access.</p> | <p>The landscape plan provides guidelines for the treatment of areas within the Artesia Corridor Specific Plan, including parkways, medians, development edges, project entries, and open space areas. The landscape plan establishes an identity and theme for the project and will act as an overall unifying element, transcending parcel boundaries and defining open space areas.</p> <p>The existing site is comprised of multiple properties owned by several individual parties. The Specific Plan will facilitate lot consolidation to prevent piecemeal development, and redevelop the site as a unified whole to increase property values and maximize the commercial potential of the project site. The Specific Plan provides for the creation of up to 415,000 square feet of commercial uses within a pedestrian-friendly mixed-use plan.</p> |
| LU Goal 3: Provide high quality, attractive and well-maintained commercial, industrial, and public environments that enhance the image and vitality of the City. | |
| <p>LU 3.1: Require adequate off-street parking, internal circulation and loading spaces for commercial developments.</p> | <p>Due to the size and classification of the surrounding roadways, the project must be able to park all users within the boundaries of the Specific Plan. The Development Standards section of this Plan identifies a minimum requirement of off-street parking spaces, including a provision for shared parking to ensure adequate parking is provided.</p> <p>The Circulation Plan (Figure 4.3a) establishes an internal main street that promotes a pedestrian friendly atmosphere while also enhancing vehicular circulation along the perimeter roadways and within the site. Access points will be strategically placed along Artesia Boulevard, Western Avenue, and Normandie Avenue to allow traffic to easily access the parking areas and the main street.</p> <p>The main street serves as the site's circulatory backbone and ensures safe and efficient access to all portions of the site for pedestrians and automobiles. The main street also distinguishes the site as a unique place to live and shop in Gardena, offering an alternative to the typical power center. Service roads are also provided at the rear of the site to allow service vehicles to access the majority of commercial uses without disturbing the traffic flow and pedestrian activity along the main street and Artesia Boulevard.</p> |



Table 7-1
General Plan Consistency Analysis

| RELEVANT POLICY | ANALYSIS OF PROJECT CONSISTENCY |
|--|---|
| <p>LU 3.3: Consider the adoption of specific plans for the four corners at the Van Ness-Rosecrans, Western-Rosecrans, Rosecrans-Normandie centers and the Artesia corridor to promote and direct revitalization efforts, and to further study and evaluate whether a mix of commercial and residential development is appropriate in these areas.</p> <p>LU 3.4: Attract commercial and industrial uses that minimize adverse impacts on surrounding land uses and are economically beneficial to the City in terms of revenue generation and employment opportunities.</p> <p>LU 3.5: Promote the development and preservation of attractive commercial and industrial development with ample landscape treatment, adequate parking and the full range of customer amenities.</p> | <p>The City has organized the Artesia Corridor Specific Plan to create a unique mixed-use development that maximizes the retail potential of the Corridor, attracts high quality residential development, and creates a special place for the City of Gardena.</p> <p>The Specific Plan allows for the creation of up to 415,000 square feet of high-quality commercial uses within a pedestrian-friendly mixed-use plan. The permitted uses within the Specific Plan are consistent with the surrounding uses.</p> |
| <p>LU 3.7: Require the mitigation or remediation of potential hazardous conditions in the City.</p> | <p>The parcel adjacent to the railroad along Normandie Avenue is contaminated. Referred to as the Gardena Sumps, these sumps were dumping grounds for waste oil sludge, rinse water, acids, and tank bottom sludge in the 1940s.</p> <p>The Specific Plan intends to stimulate development around the Sumps to increase property values and development potential to the point where remediating the site (as necessary) is financially feasible.</p> |
| <p>LU 3.8: Require all outdoor storage to be concealed from view from the public right of ways and adjoining land uses.</p> <p>LU 3.9: Require loading and unloading of materials to be conducted completely on private property and out of sight from a public street.</p> | <p>Service areas, utility areas, and trash enclosures are required and encouraged to be incorporated within the adjacent building envelope. If this is infeasible, these areas should be oriented away from public view, within design enclosures that provide ornamental screening and landscaping.</p> |
| <p>LU 3.10: Ensure new development provides adequate improvements, dedications, and fees to the City to fully cover the cost of the City services and facilities.</p> | <p>The project benefits from the ability for proposed utility lines to connect to existing infrastructure facilities. The sewage, water, drainage, and solid waste plans are designed to serve the proposed development within the Artesia Corridor Specific Plan. The project applicants will be required to pay public service impact fees to mitigate impacts on public services.</p> |
| <p>LU Goal 4: Provide the highest quality of public facilities possible to meet the needs of the City's residents and businesses and promote the City's image and cultural heritage.</p> | |
| <p>LU 4.1: Design parks and public facilities that enhance the appearance of the surrounding areas and promote the City's identity.</p> <p>LU 4.4: Utilize public easements and right of ways (flood control, power lines) for recreational, open space, and beautification purposes.</p> | <p>Because the Artesia Corridor Specific Plan will not provide a parkland space directly on the site, in-lieu fees must be paid to pay for up to 2.7 acres of parkland. The Specific Plan recommends that the in-lieu fees required for park requirements be used towards the creation of parkland directly across the Dominguez Channel.</p> <p>The area south of the Channel, identified in Figure 4.6a, is the recommended site for potential parkland and covers approximately 4.3 acres. The site is located within an Edison easement, where generally the conversion of easement to open space would be difficult. However, with the payment of in-lieu fees, the fees would directly transfer to purchase the site and convert the easement to parkland. This would allow for passive parkland used for relaxation, exercise, and other activities.</p> |



**Table 7-1
General Plan Consistency Analysis**

| RELEVANT POLICY | ANALYSIS OF PROJECT CONSISTENCY |
|--|---|
| COMMUNITY DEVELOPMENT ELEMENT—ECONOMIC DEVELOPMENT PLAN | |
| ED Goal 1: Promote a growing and diverse business community that provides jobs, goods and services for the local and regional market, and maintains a sound tax base for the City. | |
| <p>ED 1.4: Encourage high quality mixed-use development in underutilized commercial and industrial areas where it will improve the City's tax base and image.</p> <p>ED 1.5: Support regional-serving commercial development at key focus areas – Artesia Boulevard Corridor and the areas around the three intersections along Rosecrans Avenue at Van Ness, Western and Normandie.</p> | <p>The provisions of this Specific Plan will foster the creation of a recognizable, landmark development for Gardena that appeals to the City's diverse population, stands out from other development along Artesia Boulevard, and provides much-needed gathering spaces for the community.</p> <p>The Specific Plan provides up to 375,000 square feet of general commercial and 40,000 square feet of restaurant to offer local residents enhanced shopping, eating and entertainment opportunities while enhancing the City's revenue generation.</p> <p>To realize a project that goes beyond a typical power center, the Specific Plan creates a unique mixed-use environment with a combination of commercial, residential, and live/work uses oriented around a centralized main street.</p> |
| ED Goal 2: Expand, retain and revitalize quality businesses. | |
| ED 2.1: Encourage the assemblage of small commercial parcels to accommodate quality commercial development. | <p>The existing site is comprised of multiple properties owned by several individual parties. The Specific Plan will facilitate lot consolidation to prevent piecemeal development, and redevelop the site as a unified whole to increase property values and maximize the commercial potential of the project site. The Specific Plan provides for the creation of up to 415,000 square feet of commercial uses within a pedestrian-friendly mixed-use plan.</p> |
| ED Goal 3: Attract desirable businesses to locate in the City. | |
| ED 3.5: Provide incentives to developers and brokers to help attract desirable new businesses and jobs to the City. | <p>Retail analyses indicated a need for large sites to develop and attract high quality commercial uses. The Specific Plan will facilitate lot consolidation to redevelop the site as a unified whole to increase property values and maximize the commercial potential of the project site. The Specific Plan provides for the creation of up to 415,000 square feet of commercial uses within a 44-acre pedestrian-friendly mixed-use plan.</p> |
| COMMUNITY DEVELOPMENT ELEMENT—COMMUNITY DESIGN PLAN | |
| DS Goal 1: Enhance the visual environment and create a positive image of the City. | |
| DS 1.4: Provide a sense of arrival to Gardena through entry monument signs, landscaping features, architectural and motifs at key gate-way locations. | <p>The Specific Plan establishes guidelines for the design and location of Monument Gateway signs along Artesia Boulevard to signal a sense of arrival to passing motorists along this traffic corridor. The Landscape Plan also describes landscaping features for enhanced gateway areas in coordination with the placement of the Monument Gateway signs.</p> |
| DS 1.6: Require streetscape development standards for major corridors, including streetlights, landscaping, public signage and street furniture, to reinforce Gardena's community image. | <p>The Streetscape section of the Design Standards & Guidelines outline standards for streetscape elements such as signage, lighting, street furnishings, walls, fences, gates, paving materials, water features, and public art. The standards require that these elements are consistent and complementary in design throughout the project.</p> |



**Table 7-1
General Plan Consistency Analysis**

| RELEVANT POLICY | ANALYSIS OF PROJECT CONSISTENCY |
|---|--|
| DS 1.9: Provide a unique system of directional signage for vehicular and pedestrian circulation. | A Sign Program that identifies a hierarchy of signs (directional signs, center signs, etc.) with a common theme, as well as the signage location and style shall be required on a project by project basis. These Sign Programs shall be consistent and complementary within the specific plan. |
| DS 1.10: Encourage private participation in a public arts program. | The Specific Plan requires the provision of public art for new development, or expansion or remodeling of existing buildings with a building permit valuation of \$300,000 or more. |
| DS Goal 2: Enhance the aesthetic quality of the residential neighborhoods in the City. | |
| DS 2.1: Provide stronger design guidelines for residential development, including both new construction and additions to existing single-family units or multi-family dwellings. DS 2.14: Require design standards be established to provide for attractive building design features, safe egress and ingress, sufficient parking, adequate pedestrian amenities, landscaping, and proper signage. | The Design Standards and Guidelines set forth requirements for the new construction of multi-family dwellings. These include standards for building orientation, pedestrian spaces, parking areas, service areas, architectural styles and building design, and streetscape elements. |
| DS 2.2: Ensure that new and remodeled dwelling units are designed with architectural styles, which are varied and are compatible in scale and character with existing buildings and the natural surroundings. DS 2.3: Encourage a variety of architectural styles, massing, floor plans, color schemes, building materials, façade treatments, elevation and wall articulations. | Architectural styles selected for the Artesia Corridor project are required to be compatible with the surrounding architecture of the existing community. Visual diversity is promoted through the use of different architectural styles within the project, but these styles must be complementary within the project and with the existing architectural styles. Guidelines for building mass, scale, and form encourage the modulation and articulation of building form to avoid repetitive or blank elevations. |
| DS 2.4: Strengthen the important elements of residential streets that unify and enhance the character of the neighborhood, including pedestrian amenities, parkways, mature street trees, compatible setbacks, and unified architectural detailing and building. | The street sections within the Circulation Plan dictate street dimensions and setback requirements to create environments appropriate for residential, mixed use, live/work, and commercial areas within the project. As well, the Design Standards and Guidelines address streetscape elements that enhance the character of each street. |
| DS 2.5: Encourage homeowner associations and neighborhoods to maintain existing housing tract entrance signs in an attractive manner and encourage the placement of such signs at the entrance of major developments. | It is envisioned that the majority, if not all, of the properties within the Specific Plan will be managed by a property owner or homeowner association(s). Prior to the approval of any new development, the applicant shall provide a clear description of the role of the association in providing and maintaining private roadways, amenities, landscaping and other improvements. |
| DS 2.9: Integrate new residential developments with the surrounding built environment. In addition, encourage a strong relationship between the dwelling and the street. | The Specific Plan provides the vision to develop 44 acres of underutilized commercial and industrial property with a blend of mixed use, commercial, and residential uses. The development builds upon advantages offered by the location of the site, frontage along Artesia Boulevard, and the demand for commercial and residential uses. |



**Table 7-1
General Plan Consistency Analysis**

| RELEVANT POLICY | ANALYSIS OF PROJECT CONSISTENCY |
|--|---|
| <p>DS 2.10: Provide landscape treatments (trees, shrubs, groundcover, and grass areas) within multi-family development projects in order to create a “greener” environment for residents and those viewing from public areas.</p> <p>DS 2.11: Incorporate quality residential amenities such as private and communal open spaces into multi-unit development projects in order to improve the quality of the project and to create more attractive and livable spaces for residents to enjoy.</p> | <p>Open space guidelines for residential uses ensure adequate green areas for resident use. A minimum of 70 square feet per unit shall be private with a minimum dimension of 7 feet in any direction. For usable common open space, a minimum of 50 square feet per unit is required, with a minimum level surface dimension of 20 feet in any direction.</p> <p>In addition, the Landscape Plan forms landscaping requirements for these areas, ensuring their function and use for residents. The Landscape Plan requires landscaping within the project to be more urban at the community core, with softer and “greener” landscaping at the project edges.</p> |
| <p>DS 2.12: Provide well-designed and safe parking areas that maximize security, surveillance, and efficient access to building entrances.</p> | <p>Parking areas are required to be clustered where feasible, to avoid large, expansive parking lots and maximize security, surveillance, and efficient access. Lighting standards for parking areas emphasize pedestrian scale lighting to promote safety. As well, entrance and exit points are encouraged to be well marked with streetscape and landscape features for efficient access.</p> |
| <p>DS 2.13: Encourage lot consolidation for multi-family development projects in order to produce larger sites with greater project amenities.</p> | <p>The existing site is comprised of multiple properties owned by several individual parties. The Specific Plan will facilitate lot consolidation to prevent piecemeal development, and redevelop the site as a unified whole to increase property values and maximize the commercial potential of the project site. The Specific Plan provides for the creation of up to 415,000 square feet of commercial uses, along with project amenities to ensure a pedestrian-friendly mixed-use plan.</p> |
| <p>DS Goal 3: Improve the vitality of designated commercial corridors with well-designed mixed-use development that attractively blends commercial retail, office and residential uses in design and development features.</p> | |
| <p>DS 3.1: Consider the adoption of specific plans at identified focus areas that provide design and landscape standards that support either a street-oriented or village-type development pattern.</p> <p>DS 3.2: Promote pedestrian amenities in mixed-use developments and along arterials.</p> <p>DS 3.3: Promote the segregation of residential parking from commercial and office parking.</p> <p>DS 3.4: Support mixed-use developments that include adequate open space areas and a full range of site amenities.</p> <p>DS 3.5: Encourage underground parking or surface parking with effective landscape buffers to minimize the visual impact of parking areas.</p> | <p>The City has organized the Artesia Corridor Specific Plan to create a unique mixed-use development that maximizes the retail potential of the Corridor and attracts high quality residential development. The document sets forth guidelines and standards for site planning, architecture and design, landscape, and streetscape elements to produce a special place for the City of Gardena.</p> <p>Open space requirements and streetscape design elements work to create a pedestrian-friendly environment within the project. The amenities provided for the plan emphasize function and use for residents and visitors.</p> <p>Parking standards dictate the segregation of parking for the different uses within the mixed use development. Parking areas are required to be clustered where feasible, to avoid large, expansive parking lots and maximize security, surveillance, and efficient access. Underground parking is encouraged in conjunction with residential development.</p> |



Table 7-1
General Plan Consistency Analysis

| RELEVANT POLICY | ANALYSIS OF PROJECT CONSISTENCY |
|---|--|
| DS Goal 4: Achieve high quality design for commercial uses. | |
| <p>DS 4.1: Promote a strong relationship between buildings and the street.</p> <p>DS 4.2: Provide functional pedestrian connections between adjacent commercial uses.</p> <p>DS 4.3: Encourage commercial developments to include interesting rooflines, building shapes, and patterns of shade and shadow while demonstrating sensitivity to the contextual influences of the surrounding area and compatibility with surrounding neighbor-hoods.</p> <p>DS 4.5: New or remodeled commercial structures and properties should be designed to reflect the City's architectural diversity, yet be compatible with nearby existing buildings.</p> <p>DS 4.6: Promote pedestrian-friendly corridors by improving traffic and pedestrian safety and by providing pedestrian amenities such as benches and outdoor seating, potted plants, decorative paving, and detailed lighting elements along the street frontage.</p> <p>DS 4.7: Encourage the use of entrance patios, courtyards, plazas, arcades, fountains, porches, tower elements, covered walks, and other design features and amenities in commercial areas.</p> <p>DS 4.8: Require loading areas, access and circulation driveways, trash and storage areas, and rooftop equipment to be concealed from view and located as far as possible from adjacent residences.</p> <p>DS 4.9: Reduce the impact of monotonous walls with vertical and horizontal design elements or landscaping.</p> <p>DS 4.10: Encourage the design of highly visible entrances to retail activity centers through accent landscaping, water features, sculptures, enhanced intersection features, façade detailing, monument signs, water features, and other design amenities.</p> <p>DS 4.11: Nodes within the City should be designated as community focal points with heightened design quality and visual interest.</p> <p>DS 4.12: Enhance the physical appearance of the industrial and commercial areas through better "edge" identification, signage and landscaping.</p> | <p>The street sections within the Circulation Plan dictate street dimensions and setback requirements to shape the environment between building and street. The Design Standards and Guidelines address streetscape elements that enhance the character of each street appropriate for the different areas within the project.</p> <p>The Design Standards and Guidelines section of the Specific Plan shape the site design and architecture of the project, in addition to directing streetscape components. The site design for the project sets forth standards for building orientation and site planning, including the treatment of points of interest within the project. As well, treatment and location of parking areas, pedestrian pathways, and service areas are addressed to promote a pedestrian-friendly environment.</p> <p>Service areas, utility areas, and trash enclosures are required and encouraged to be incorporated within the adjacent building envelope. If this is infeasible, these areas should be oriented away from public view, within design enclosures that provide ornamental screening and landscaping.</p> <p>Architectural styles selected for the Artesia Corridor project are required to be compatible with the surrounding architecture of the existing community. Visual diversity is promoted through the use of different architectural styles within the project, but these styles must be complementary within the project and with the existing architectural styles. Guidelines for building mass, scale, and form encourage the modulation and articulation of building form to avoid repetitive or blank elevations. Additional architectural guidelines focus on design details, shadow elements, rooflines variations, and accent features such as towers to ensure architectural interest.</p> <p>The streetscape components addressed within the Design Standards and Guidelines section set forth requirements for signage, lighting, special paving, walls and fences, and street furnishings such as seating areas and shade structures. The treatment and use of these elements allow for focal points within the community, and promote a pedestrian-friendly environment.</p> |



**Table 7-1
General Plan Consistency Analysis**

| RELEVANT POLICY | ANALYSIS OF PROJECT CONSISTENCY |
|--|--|
| DS Goal 6: Ensure that signage contributes positively to Gardena's image and to the overall economic development. | |
| <p>DS 6.1: Encourage the use of common design elements in signs for commercial and industrial centers.</p> <p>DS 6.2: Prohibit the use of pole signs, roof signs, temporary lettering of window signs, banner signs and temporary signs.</p> <p>DS 6.3: Wall signs along a continuous façade of storefronts shall be located at approximately the same height to provide a unifying, horizontal design element.</p> <p>DS 6.4: Consistent design style should be established for all public signage, including fixture type, lettering color, symbols and logos.</p> | <p>The requirements for signage within the Design Standards and Guidelines section of the Specific Plan relate to different levels of signage including monument gateway signs, directional signs, retail monument signs, and building identification signs. General standards include requirements such as the consistent design of signs throughout the project, and restraints such as the prohibition of pole signs.</p> <p>In addition to signage requirements within the Specific Plan, a Sign Program that identifies a hierarchy of signs (directional signs, center signs, etc.) with a common theme, as well as the signage location and style shall be required on a project by project basis. These Sign Programs shall be consistent and complementary within the specific plan.</p> |
| DS Goal 7: Utilize extensive landscaping to beautify Gardena's streets and sidewalks. | |
| <p>DS 7.1: Pursue unifying streetscape elements for major corridors, including landscaping, public signage, banners, streetlights, and street furniture to foster the City's streetscape.</p> <p>DS 7.2: Provide landscaped medians on Rosecrans Avenue, Western Avenue and Redondo Beach Boulevard.</p> <p>DS 7.3: Provide opportunities to incorporate public art as part of new development projects to enhance the public streetscape or to incorporate into public improvement projects.</p> <p>DS 7.4: Screen or underground unsightly equipment cabinets, infrastructure support structures and equipment.</p> <p>DS 7.5: Underground electric and communication lines.</p> <p>DS 7.6: Require consistent landscaping character along streets that reinforces the unique qualities of each corridor and neighborhood.</p> <p>DS 7.7: Maintain landscape parkways in residential areas and decorative parkways in commercial and industrial areas.</p> | <p>The Landscape Plan outlined within the Specific Plan provides for a consistent and complementary landscape theme throughout the project. Landscape elements are addressed for areas within the project, including the median and enhanced gateway areas along Artesia Avenue, landscaping for the different streets within the project, and landscaping for parking areas and public spaces.</p> <p>In addition to landscaping, streetscape elements such as signage, lighting, seating areas, and shade structures are addressed in the Design Standards and Guidelines section to reinforce the image of the project.</p> <p>The Design Standards and Guidelines also require exterior on-site facilities, including sewer, gas, water, electric, telephone, and communications equipment be installed underground where feasible. Transformers and other utility equipment that must be above ground should be screened and incorporated into the landscape wherever possible.</p> |



**Table 7-1
General Plan Consistency Analysis**

| RELEVANT POLICY | ANALYSIS OF PROJECT CONSISTENCY |
|---|--|
| COMMUNITY DEVELOPMENT ELEMENT—CIRCULATION PLAN | |
| CI Goal 1: Promote a safe and efficient circulation system that benefits residents and businesses, and integrates with the greater Los Angeles/South Bay transportation system. | |
| CI 1.1: To the extent feasible, maintain traffic flows at nonresidential, signalized intersections at LOS D, and maintain LOS E during peak rush hours. | <p>There are currently a 17 access points to the project site. Only one access point off of Artesia Boulevard is a signalized intersection. Each one of the unsignalized driveways can act as a point of conflict, as traffic must slow down along Artesia Boulevard to allow vehicles to enter or exit the project site. Additionally, internal access opportunities are unavailable, so travelling from one property to another requires vehicles to reenter Artesia Boulevard, creating additional traffic and congestion.</p> <p>The Specific Plan provides for ten consolidated points of access to provide safe and efficient entry to and exit from the site, and to enhance traffic flow and safety along Artesia Boulevard. Three signalized access points are located along Artesia Boulevard and aligned to access points directly opposite, along the northern side of Artesia Boulevard. There are also seven secondary right-in/out access points off of the surrounding major roads to transfer traffic into the project.</p> |
| CI 1.4: Provide streetscape enhancement programs for major highways, to improve the appearance of streets. | The Specific Plan provides design standards and guidelines to enhance the appearance of Artesia Boulevard, Western Avenue, and Normandie Avenue. |
| CI Goal 2: Promote a safe and efficient local street system that is attractive and meets the needs of the community. | |
| CI 2.5: Traffic-calming measures and devices (e.g., sidewalks, streetscapes, speed humps, traffic circles, cul-de-sacs and signals) should promote safe routes through neighborhoods for pedestrians. | The Specific Plan encourages the use of traffic calming measures within the project site to ensure the safety of pedestrians. |
| CI 2.6: Provide signs at major City gateways to indicate arrival into the Gardena and to indicate the direction to heavily frequented destinations within the City. | The Specific Plan provides for the design of major entries to create a sense of entrance and arrival through the use of landscape design, themed signage, lighting, and hardscape elements. |
| CI Goal 3: Promote alternative modes of transportation that are safe and efficient for commuters, and available to persons of all income levels and disabilities. | |
| CI 3.1: Work with Gardena Municipal Bus Lines and MTA to increase the use of public transit, establish or modify routes, and improve connectivity to regional services. | With transit lines running along the three major streets that border the Artesia Corridor Specific Plan, the site benefits from the ability to use mass transit. The Specific Plan creates a concentration of residential and commercial uses along these transit lines to attract additional transit service. |
| CI Goal 4: Provide adequate public facilities and infrastructure that support the needs of City residents and businesses | |
| CI 4.2: A comprehensive plan to finance the ongoing maintenance, repair, and rehabilitation of City infrastructure systems. | Several mechanisms (reimbursement agreement and public financing mechanisms) area described and outlined to fund public improvements necessary to develop the Artesia Corridor Specific Plan. |



Table 7-1
General Plan Consistency Analysis

| RELEVANT POLICY | ANALYSIS OF PROJECT CONSISTENCY |
|---|---|
| COMMUNITY RESOURCES ELEMENT— OPEN SPACE PLAN | |
| OS Goal 1: Maintain and upgrade the existing parks and recreation facilities to meet the needs of all residents. | |
| OS 1.7: Promote creative financing mechanisms to fund the development and maintenance of parks and recreation programs, such as State grant funds, park in-lieu fees, and public private partnerships. | Because the Artesia Corridor Specific Plan will not provide a parkland space directly on the site, in-lieu fees must be paid to pay for up to 2.7 acres of parkland. The Specific Plan recommends that the in-lieu fees required for park requirements be used towards the creation of parkland directly across the Dominguez Channel. |
| OS Goal 2: Increase the City’s supply and quality of parkland, open space, and recreational programs. | |
| OS 2.3: Encourage the conversion of utility easements and right of ways to multi-purpose parkland, trails and bicycle routes (i.e., the Southern California Edison right-of-way between Artesia Boulevard and 178th Street, | The area south of the Channel, identified in Figure 4.6a, is the recommended site for potential parkland and covers approximately 4.3 acres. The site is located within an Edison easement, where generally the conversion of easement to open space would be difficult. However, with the payment of in-lieu fees, the fees would directly transfer to purchase the site and convert the easement to parkland. This would allow for passive parkland used for relaxation, exercise, and other activities. |
| COMMUNITY SAFETY ELEMENT—PUBLIC SAFETY PLAN | |
| PS Goal 1: Maintain a high level of fire and police protection for residents, businesses and visitors. | |
| PS 1.2: Provide funding levels necessary for high level of police, fire protection, and building code enforcement. | The project applicants will be required to pay public service impact fees to mitigate impacts on public services. |
| PS 1.5: Require that adequate police and fire service facilities and personnel be maintained to provide services at sufficient levels. | |
| PS Goal 4: Increase public awareness of crime and fire prevention, and emergency preparedness and procedures | |
| PS 4.3: Promote professional management of multi-family residential buildings. | It is envisioned that the majority, if not all, of the properties within the Specific Plan will be managed by a property owner, business owner or homeowner association(s). However, all of the roadways will be privately maintained by one or more of the owner associations. Prior to the approval of any new development, the applicant shall provide a clear description of the role of the association in providing and maintaining private roadways, amenities, landscaping and other improvements. |
| COMMUNITY SAFETY ELEMENT—NOISE PLAN | |
| N Goal 1: Use noise control measures to reduce the impact from transportation noise sources. | |
| N 1.1: Minimize noise conflicts between land uses and the circulation network, and mitigate sound levels where necessary or feasible to ensure the peace and quiet of the community. | The Circulation Plan establishes an internal main street that promotes a pedestrian friendly atmosphere while also enhancing vehicular circulation along the perimeter roadways and within the site. The Specific Plan features a encourages the use of traffic calming measures, such as on-street parking along a narrow main street, within the project site to ensure the safety of pedestrians. |
| N 1.4: Promote the use of traffic calming measures where appropriate, such as narrow roadways and on street parking, in commercial and mixed-use districts. | |



**Table 7-1
General Plan Consistency Analysis**

| RELEVANT POLICY | ANALYSIS OF PROJECT CONSISTENCY |
|--|---|
| <p>N 1.8: Encourage walking, biking, carpooling, use of public transit and other alternative modes of transportation to minimize vehicular use and associated traffic noise.</p> | <p>With transit lines running along the three major streets that border the Artesia Corridor Specific Plan, the site benefits from the ability to use mass transit. The Specific Plan creates a concentration of residential and commercial uses along these transit lines to attract additional transit service. Additionally, the land use plan is designed to facilitate pedestrian activity and preclude the need for residents and visitors to use a car to access other portions of the site.</p> |
| N Goal 2: Incorporate noise considerations into land use planning decisions. | |
| <p>N 2.2: Require noise/land use compatibility standards to guide future planning and development.</p> <p>N 2.4: Require mitigation of all significant noise impacts as a condition of project approval.</p> <p>N 2.6: Require new residential developments located in proximity to existing commercial/ industrial operations to control residential interior noise levels as a condition of approval and minimize exposure of residents in the site design.</p> <p>N 2.8: Require that mixed-use structures and areas be designed to prevent transfer of noise and vibration from commercial areas to residential areas.</p> | <p>The Project would include feasible development standards and mitigation measures to ensure compatibility between land uses and address potentially significant impacts regarding Noise.</p> <p>The Specific Plan provides that no use, activity or process shall produce continual vibrations or noxious odors that are perceptible without instruments by the average person at the property lines of the site or within the interior of residential units on the site.</p> |
| Source: The Planning Center, 2006. | |



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8. ADMINISTRATION AND IMPLEMENTATION

8.1 GENERAL

The Artesia Corridor Specific Plan and its provisions shall be implemented as the zoning for the site. In the event of a conflict between the provisions of the Artesia Corridor Specific Plan and the provisions identified in the City of Gardena Municipal Code (2006), the Specific Plan shall prevail. If the Specific Plan is silent regarding to any development standard or process, the provisions identified in the Municipal Code shall prevail.

8.1.1 Interpretation

In case of uncertainty or ambiguity to the meaning or intent of any provision of this Specific Plan, the Community Development Director has the authority to interpret the intent of the provision. The Director may, at his/her discretion, refer interpretations to the Planning Commission for consideration and action. Such a referral shall be accompanied by a written analysis of issues related to the interpretation. All interpretations made by the Community Development Director may be appealed to the Planning Commission in accordance with the appeal procedures set forth in Title 18 of the Gardena Municipal Code (2006).

8.1.2 Severability

If any section, subsection, sentence, clause, or phrase of this Specific Plan, or future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court, such decision shall not affect the validity of the remaining portions of the plan.

8.1.3 Approval Authority

The Community Development Director's responsibilities shall include administering, interpreting, and enforcing all requirements and standards of the Artesia Corridor Specific Plan, including the acceptance and processing of all land use permit applications. The Community Development Director or designated representative may approve administrative permits that meet the requirements of this Specific Plan and may approve, conditionally approve, or deny minor requests for waiver of Specific Plan standards. The Community Development Director holds final approval authority for, and enforcement of: building permits, certificates of occupancy, home occupation permits, sign permits, and temporary use permits.

The Planning Commission may approve, conditionally approve, or deny Conditional Use Permits, applications for major modifications, and any item referred to the Commission by the Community Development Director. The City Council will have approval authority over Specific Plan Amendments and appeals to Planning Commission actions.

8.1.4 Environmental Review

The Environmental Impact Report (EIR) serves primarily as a source of environmental information for the City of Gardena as lead agency for the project. The EIR describes the potential impacts that could result from the adoption of the Artesia Corridor Specific Plan. Subsequent development projects within the Artesia Corridor Specific Plan are anticipated. While the EIR has been prepared as a project EIR, as defined by §15161 of the CEQA Guidelines, subsequent projects that are within the scope of this EIR may be subject to a more limited environmental review process if that is determined to be necessary by the Director of Community Development of the City of Gardena.



8.2 REVIEW AND APPROVAL PROCESS

8.2.1 Pre-Application Conference

A pre-application conference with the Community Development Director or his/her designee should be held before a proposed project can be submitted and accepted for processing. Pre-application conferences are recommended for conceptual Master Plans, Administrative Development Review and Conditional Use Permits.

8.2.2 Conceptual Master Plan

Because the successful implementation of the Artesia Corridor Specific Plan is dependent on the careful planning and synergistic relationship of the uses permitted in the project area, it will be extremely important to ensure that, as projects are developed at different times and phases, all new development is thoughtfully integrated into existing or proposed development in adjacent planning areas.

Additionally, the Artesia Corridor Specific Plan project area is comprised of multiple properties owned by several different individuals. The City strongly encourages property owners to work together to design and develop entire planning areas at one time so that a comprehensive plan can be created.

It is recognized that individual property owners may want to develop their properties at differing times. To prevent piecemeal development within the Specific Plan, a conceptual Master Plan shall be required for the entirety of a planning area, even if one or more property owners in that planning area is not interested in developing their property at the same time. The City recognizes that the preliminary planning provided through the Master Plan process for these areas will be conceptual in nature and will not bind a particular property to a specific mix of uses.

However, since the internal circulation system is integral to the efficient use and interconnectivity of properties within the Specific Plan area, it is likely that the location and design of internal roadways in each planning area will be influenced by the first property owner to submit a proposed development and conceptual Master Plan for the area. While proposed uses in subsequent applications may vary from those identified in the conceptual Master Plan, the fundamental location and design of the circulation system will most likely remain substantially the same as proposed in the conceptual Master Plan design, since any subsequent development in that planning area must link up to the roadways established in adjacent properties (with previously approved entitlements).

As a result, conceptual Master Plans will be required prior to the submittal of any entitlements proposed within a particular planning area. Master Plans should outline the key features and characteristics of the development proposed in a particular planning area and how that development will relate to the existing, proposed, or potential development within the subject planning area or in adjacent planning areas. In general, information needs to be included in a Master Plan so that the basic components, features and characteristics of the overall project can be clearly understood.

In particular, the conceptual Master Plan will be used to evaluate and provide preliminary feedback on the proposed circulation plan, building orientation, mix of uses, and relationship to other uses within the Artesia Corridor Specific Plan. To achieve the following objectives, conceptual Master Plans are required for each planning area.

1. Ensure that applicable concepts, requirements and provisions of the Artesia Corridor Specific Plan are implemented in a cohesive and comprehensive manner.



2. Establish continuity, compatibility and connectivity within the planning area, neighboring developed properties, and areas that may be developed in the future.
3. Provide for a high level of quality, character and variety of development within the planning area and throughout the Specific Plan.
4. Maximize the development value of all parcels within the Specific Plan and prevent the creation of orphaned parcels.
5. Preliminary verification that the proposed uses fall within the prescribed thresholds for commercial square footage outlined in Section 4.2.2.

Similar to the Administrative Development Review process (identified below), the Community Development Director has the authority to administratively review and approve conceptual Master Plans. Any action taken by the Community Development Director (administrative approval, conditional approval, or denial of a conceptual Master Plan) shall be presented to the Planning Commission via Study Session. No project can be considered for review or approval by the Community Development Director or Planning Commission prior to the approval of a conceptual Master Plan.

8.2.3 Administrative Development Review

Applications that comply with the provisions of the Specific Plan and do not require the approval of a Conditional Use Permit may be approved or conditionally approved by the Community Development Director through the Administrative Development Review process. Once the Community Development Director has taken administrative action on a Site Plan, the applicant, the owner or owners of the property subject to the Development Plan, and the Planning Commission shall be promptly notified of the Community Development Director's decision.

The decision of the Community Development Director shall be final and effective 14 days after a written determination has been made unless, within said time, a written appeal to the Planning Commission is filed by the applicant, property owners subject to the Development Plan, or by any member of the City Council or Planning Commission.

Appeals from a decision or action taken by the Community Development Director may be appealed to the Planning Commission. Actions or decisions made by the Planning Commission to approve or disapprove an application or to grant or revoke a Conditional Use Permit or Administrative Development Review may be appealed to the City Council, and may be filed by any person, subject to the requirements of Title 18. A filing fee shall be paid at the time of appeal application, in an amount determined by the City Council.

The approval of a project subject to Administrative Development Review is conditional upon the privileges being utilized within six months after the effective date, and if they are not utilized or construction work is not begun within this time and carried on diligently in accordance with conditions imposed by the Planning Commission, this authorization shall become void, and any privileges, permit or conditions granted shall have elapsed. Upon written request, the Community Development Director may grant an extension of an additional six months if the applicant can show significant progress on the project. Approved projects that will be carried out in two or more phases are not required to begin all phases of work within the six month approval window.



8.2.4 Conditional Use Permits

Uses listed in this Specific Plan as being permitted subject to a Conditional Use Permit may be permitted when such uses are:

- A. Necessary to the development of the community and in the best interests of the public convenience; and
- B. In no way detrimental to the public health, morals, safety and welfare of the community nor in any way injurious to property or improvements in the neighborhood.

Uses requiring the approval of a Conditional Use Permit are identified on Table 5-1a in this Specific Plan. Uses in this Specific Plan requiring a Conditional Use Permit shall be subject to the filing, notification, hearing, and appeal procedures identified in Section 18.46 of the Gardena Municipal Code (2006). Uses shall be subject to the findings required for Conditional Use Permits in the Municipal Code, as well as the supplemental findings required as part of this Specific Plan (Section 8.2.12).

8.2.5 Home Occupation Permits for Live/Work Units

The purpose of a Home Occupation Permit is to allow certain “work” uses to be conducted by the residents within a live/work unit, as identified on Table 5-1a of this Specific Plan, without the need for a Conditional Use Permit. The Home Occupation Permit ensures that “work” uses are incidental and secondary to the residential use and are compatible with the area in which the dwelling is located. A live/work use may not require a home occupancy permit if the “work” component does not require a business license. A home occupation permit for live/work uses may be granted only if all the following conditions are met:

- A. There shall be no generation of traffic, parking, sewerage, or water use in excess of what is identified in the Artesia Corridor Specific Plan and analyzed in the associated Environmental Impact Report;
- B. The home occupation shall not create a hazard to persons and/or property or become a nuisance;
- C. No required parking space, either in a garage or otherwise, shall be utilized to conduct the home occupation or to store materials related to the home occupation;
- D. The live/work unit complies with Building, Fire, Planning, Public Works, and Police requirements; and
- E. The Community Development Director may impose such additional conditions as deemed necessary to safeguard the health, safety and general welfare of uses in the Specific Plan.

The procedure for obtaining a home occupation permit shall be as follows:

- A. An application for a home occupation permit shall be filed with the Community Development Department. As a part of the application, the applicant, if not the owner of the property, shall furnish a written statement from the property owner or his/her representative agreeing to the conduct of the subject business.
- B. The applicant shall pay to the City a filing fee established by City Council resolution to defray the costs incurred by the City in processing the home occupation permit application.



- C. Within 30 days after the date on which the application was filed, the Community Development Director or his/her designee shall review the application to determine if the proposed use meets all of the requirements set forth in this Specific Plan. If it is determined that all such requirements are met, the Community Development Director shall approve, conditionally approve or deny the permit.

The decision of the Community Development Director or his/her designee may be appealed by the applicant or an affected person to the Planning Commission and City Council. Such appeal shall be filed with the Community Development Department within 10 days after the date of the decision by the Community Development Director. Upon the receipt of such an appeal, the Community Development Director shall place the matter on the Planning Commission agenda for consideration at the first regular meeting of the Commission following 10 days written notice to the appellant or applicant. The Commission shall either approve the application with conditions or deny the application based on its findings. The decision of the commission shall be final unless it is appealed to the Council pursuant to the provisions of Chapter 18.72 of the Municipal Code (2006).

8.2.6 Temporary Use Permits

Temporary Use Permits identified in Table 5-1a of this Specific Plan shall be subject to the provisions of Title 18.60 of the Municipal Code (2006).

8.2.7 Substantial Conformance

Substantial Conformance allows for administrative approval and interpretation of minor modifications to the Specific Plan text, graphics, and/or project design that do not change the meaning or intent of the Specific Plan. Through the Administrative Review or Conditional Use Permit process, a project may be found to be in substantial conformance with the provisions of this Specific Plan and may be approved, conditionally approved or denied by the Community Development Director. The Community Development Director shall also have the discretion to refer any such request for Substantial Conformance to the Planning Commission. The following are examples of issues that may be considered for Substantial Conformance:

- A. Realignment or modifications of streets serving the project, lot lines, easement locations, and grading adjustments, if also approved by the City Engineer.
- B. Modification of design criteria such as paving treatments, architectural details, landscape treatments, sidewalks, fencing, lighting, and entry treatments.
- C. Changes to the Phasing Plan, provided the City Engineer determines that infrastructure is available to serve the phase.
- D. Reductions in minimum required commercial square footage for each planning area that do not exceed 10 percent of the prescribed requirement.
- E. Variations in setbacks that do not exceed 10 percent of the prescribed dimension.

8.2.8 Variances

Where practical difficulties or unnecessary hardships would occur as a result of the strict interpretation and application of the provisions of this Specific Plan, a variance may be considered subject to the requirements and findings of Section 18.48 of the Gardena Municipal Code (2006). The Planning



Commission shall make a finding that in the evidence presented all four of the following conditions exist in reference to the property being considered prior to granting a variance:

- A. That because of special circumstances applicable to the subject property, including size, shape, topography, location, or surroundings, the strict application of the provisions of this title would deprive the subject property of privileges enjoyed by other properties in the vicinity and under identical zone classifications;
- B. That any variance granted shall be subject to such conditions as will assure that the adjustment thereby authorized shall not constitute a grant of special privileges inconsistent with the limitations upon other properties in the vicinity and zone in which the subject property is situated;
- C. That the granting of the variance will not be materially detrimental to the public health, safety, convenience and welfare or injurious to property and improvements in the same vicinity and zone in which the subject property is situated; and
- D. That the granting of such variance will not adversely affect the General Plan of the city.

8.2.9 Specific Plan Amendments

Approval of this Specific Plan indicates acceptance by the City Council of a general framework for community development. Part of that framework establishes specific development standards that constitute the zoning regulations for the Artesia Corridor Specific Plan. It is anticipated that certain modifications to the Specific Plan text, exhibits, and/or project may be necessary during the development of the project. Any modifications to the Specific Plan shall occur in accordance with the Specific Plan Amendment process and are required to be reviewed for approval by the Planning Commission and the City Council. In all cases, Specific Plan Amendments must be found to be in conformance with the objectives and intent of the Artesia Corridor Specific Plan.

Amendments may be requested at any time pursuant to §65453(a) of the Government Code. Depending upon the nature of the proposed Specific Plan Amendment, a supplemental environmental analysis may be required, pursuant to the California Environmental Quality Act (CEQA), §15162.

Any Specific Plan Amendment initiated by an applicant shall require preliminary review by the Community Development Director, filing of an official application and required materials supporting the amendment, and submittal of a fee deposit prior to Planning Commission and City Council review.

8.2.10 Nonconforming Uses

The Artesia Corridor site is currently comprised of a mix of industrial, commercial, and residential development. Implementation of this Specific Plan would deem the existing industrial and residential uses nonconforming to the provisions of the Specific Plan. To contribute to the improvement of living conditions and economic vitality in this area, and to ensure that land uses are consistent with the goals, policies, and programs of the General Plan, the conditions and period under which nonconforming uses may continue is limited.

Nonconforming uses shall be subject to the provisions of Section 18.54.040 of the Gardena Municipal Code (2006), which prevents nonconforming uses from being changed or expanded beyond what was



established when the use became nonconforming. If a nonconforming use ceases to operate or exist for a period of 180 days, it will be considered abandoned or discontinued.

8.2.11 Submittal Requirements

Projects subject to Administrative Development Review or a Conditional Use Permit shall be required to submit a site plan as part of any entitlement application. The site plan shall contain the following:

1. Dimensions, shape and orientation of the parcel(s);
2. Placement of buildings and structures on the parcel(s);
3. Height, setbacks, bulk and building materials;
4. Distance between buildings or structures;
5. Location, number and layout of off-street parking and loading spaces;
6. Internal vehicular patterns and pedestrian safety features;
7. Location, amount and nature of landscaping, open space, walls and fences;
8. Placement, height and direction of illumination of lighting features;
9. Location, number, size and height of signs (a subsequent Master Sign Program shall be provided outlining the construction details of signs proposed for the application);
10. Location and method of screening refuse and storage areas, roof equipment, pipes, vents, utility equipment and all equipment not contained in the main buildings of the development;
11. Provisions for property owner or homeowner association(s) where that method of management is proposed;
12. Documentation identifying the existing and proposed square footage and/or units in the specified planning area and verification that the proposed project is within the commercial and residential thresholds as set forth by this Specific Plan;
13. Copy of an approved conceptual Master Plan; and
14. Other information which the community development director may require to make the necessary findings that the provisions of this Specific Plan are met.

8.2.12 Required Findings

Applications for new projects within the Artesia Corridor Specific Plan area may be approved or conditionally approved if it is determined that the project can, based upon the application, plans, and materials submitted, meet the following Specific Plan objectives (where applicable). Overall, the project:

1. Proposes a cohesive, complementary mix of uses structured around a comprehensive set of circulation and infrastructure systems.
2. Creates a successful mixed-use development that maximizes the commercial potential of the project site.
3. Incorporates a variety of commercial and retail opportunities, such as shopping, and entertainment activities to serve the population and increase revenues to the City of Gardena.



4. Applies innovative planning and design solutions to create a sense of place at multiple scales.
5. Facilitates lot consolidation and redevelop the site as a cohesive area to prevent piecemeal development and increase property values of the existing, underutilized commercial and industrial properties.
6. Provides new housing opportunities for the community, encouraging high-quality, high-density residential units that appeal to existing residents looking for “move-up” housing within the City and to young professionals seeking shorter commutes to jobs, restaurants, and entertainment opportunities.
7. Establishes a welcoming gateway to the City characterized by distinct and attractive signage, architecture, and landscaping, both on-site and in the public right-of-way.
8. Implements a main street concept that optimizes circulation for both vehicular and pedestrian traffic, internalizes pedestrian activity to buffer it from the vehicular traffic along Artesia Boulevard, and establishes connectivity between uses infused with pedestrian-friendly and walkable spaces.
9. Meets minimum commercial development thresholds established for each planning area, and for the project as a whole.
10. Does not exceed maximum residential development thresholds established for each planning area, and for the project as a whole.
11. Does not exceed maximum development thresholds established for the project as a whole.

8.3 MAINTENANCE AND MONITORING

A. Owner Associations

1. It is envisioned that the majority, if not all, of the properties within the Specific Plan will be managed by a property owner, business owner or homeowner association(s). However, all of the roadways will be privately maintained by one or more of the owner associations.
2. Prior to the approval of any new development, the applicant shall provide a clear description of the role of the association in providing and maintaining private roadways, amenities, landscaping and other improvements.

B. Parking Management

1. Parking management in the Specific Plan will entail a combination of providing physical spaces for parking and managing those spaces so that they are properly allocated to and used by residents, visitors, and businesses. Parking management can help prevent situations such as permanent residents using guest parking spaces instead of the enclosed parking spaces allocated to them.
2. For developments using shared parking, a tracking system shall be established to determine whether a new or revised shared parking study is necessary due to changes in tenants or uses.
3. Covenants, conditions and restrictions (CC&R's) for the Artesia Corridor Specific Plan shall address the shared nature of parking for the overall project. It is the intent that



residential uses proposed within the Specific Plan Area would be allowed to use parking spaces within commercial areas in off-peak hours. Mechanisms shall be instituted in the owner association's CC&R's that clearly define this relationship and how the parking will be managed throughout the project.

8.4 PHASING PLAN

The project is envisioned to build out in two phases. Approximately 200 units (including 45 live/work), 75,000 square feet of commercial, and 15,000 square feet of restaurant are expected to be constructed in Planning Area 3 and portions of Planning Areas 2 and 4 by 2010. The remaining portions of Planning Areas 2 and 4, as well as Planning Areas 1 and 5, could be built out by 2020. This second phase represents approximately 300,000 square feet of commercial, 25,000 square feet of restaurant, and the remaining 100 residential units (including up to 25 live/work units). Planning Area 6 is assumed to remain unchanged and continue operation as the Dominguez Channel and utility and railroad easements, although access easements will need to be acquired for service roads and potential bridges across the Channel.

| Table 8-4 Phasing Plan | | | | |
|---|---------------|------------------------------|---|----------------|
| PHASE | BUILDOUT YEAR | UNITS | SQUARE FEET | PLANNING AREAS |
| 1 | 2010 | 200 (including 45 live/work) | 75,000 commercial 15,000 restaurant | 2, 3, 4 |
| 2 | 2020 | 100 (including 25 live/work) | 300,000 commercial 25,000 restaurant | 1, 2, 4, 5 |
| Source: The Planning Center, 2006. | | | | |

8.5 FINANCING MEASURES

Several mechanisms will need to be considered to fund the public improvements necessary to develop the Artesia Corridor Specific Plan. These improvements include, but are not limited to, traffic signals, access driveways and drive aisles, and median improvements. Some of the public improvements are incremental in nature and can be borne by an individual developer. Other public improvements benefit the project as a whole and may need to be constructed and paid for prior to buildout of the Specific Plan. In these instances, the City must create a mechanism by which each property owner and developer pays their fair share cost of the public improvements. The construction and maintenance of any public improvements associated with the project should be paid for by private developers and/or the property owners through direct payment and/or the creation of a financing district.

8.5.1 Reimbursement Agreement

One method to distribute and capture costs for larger public improvements is to create a reimbursement agreement. A reimbursement agreement is a contract between the City, property owners, and/or developers under which parties can be reimbursed for funding public improvements that benefit properties beyond their own and/or are greater in size than would be required by their development.

An example of such an improvement is the creation of a new signalized intersection. Although one developer may pay the initial costs of installing the new intersection to create access to their own property, the new intersection is necessary for the full buildout of the Specific Plan and will enhance



access for adjacent property owners within the Specific Plan. Accordingly, the developer would seek to be reimbursed for the costs of installing the new intersection, less their own fair share portion of the costs.

To create a reimbursement agreement, the City will need to first perform an analysis to determine the total costs of public improvements required for the Artesia Corridor Specific Plan. Then, a fair share analysis will need to be conducted to determine the distribution of costs for each planning area and type of development. Both of these analyses will form the basis for a reimbursement agreement, to be created and managed by the City.

8.5.2 Public Financing Mechanisms

A variety of public debt financing mechanisms are available to fund public improvements in California. The advantage of most of these programs is that they provide tax-exempt financing at lower interest rates than are available through construction loans and other forms of conventional financing. Based on the City's concerns regarding the imposition of additional burdens on the City's General Fund and existing taxpayers, and the absence of a redevelopment agency, the available financing mechanisms were limited to those that would be solely the responsibility of the property owners located within the Specific Plan itself:

- Community Facilities Districts
- Special Assessment Districts
- Infrastructure Financing Districts

These three types of financing mechanisms are all land-secured financing districts that would rely solely on property taxes collected from new development in the Specific Plan and would not be backed by the City's General Fund. Each of these three mechanisms is summarized in Table 8-5, including the function, eligible activities, formation and voting requirements, and advantages and disadvantages of utilizing each of them to finance public facilities within the Specific Plan.

The City of Gardena maintains an existing Landscape and Lighting District for the non-residential properties fronting Artesia Boulevard. Through this District, funds have been generated to construct a landscaped median on portions of Artesia Boulevard. Each property owner will be expected to contribute their fair share to the Landscape and Lighting District to finance additional improvements. The boundaries of the District may be expanded to cover improvements to Western Avenue and Normandie Avenue.

In addition to these public financing techniques, it will be necessary for private sector financing to fund the construction of a portion of the public facilities required under the Specific Plan. It is the responsibility of the developer to complete each proposed improvement, irrespective of the availability of public financing.

Table 8-5
Public Debt Financing Mechanisms

| FINANCING MECHANISM | FUNCTION | ELIGIBLE ACTIVITIES | FORMATION AND VOTING REQUIREMENTS | OTHER NOTES |
|-------------------------------------|--|--|---|---|
| Community Facilities District (CFD) | A CFD permits cities, counties, and special districts to create defined areas within their jurisdiction and impose special taxes to pay for the public improvements and services needed in that area. | <p>CFDs have broad powers and can finance a wide range of public facilities and services including libraries, parks, open space, public administration buildings, police and fire stations and protection, and schools as well as roads, sewer, water, and storm drain improvements.</p> <p>CFDs can also finance certain services such as police and fire protection, recreation programs, library services, park and open space maintenance, storm drain maintenance, and school facilities maintenance.</p> | <p>A CFD can only be established with the support of two-thirds of the qualified electors within the CFD.</p> <p>If fewer than 12 registered voters reside within the proposed district boundaries, the formation of a CFD requires an election of property owners.</p> <p>More than 12 registered voters requires an election of registered voters.</p> <p>Once approved, a Special Tax Lien is placed against each property in the CFD. Property owners then pay a Special Tax each year. If the project cost is high, municipal bonds can be sold by the CFD to provide the initial money needed to build the improvements or fund the services.</p> | <p>PRO: CFD bonds permit the funding of infrastructure as needed, unlike most pay-as-you-go mechanisms such as development impact fee programs.</p> <p>PRO: CFD bonds are land secured obligations, so the City's General Fund and taxing authority are not at risk.</p> <p>PRO: CFD bonds are tax-exempt, resulting in interest rates that are lower than conventional financing.</p> <p>CON: Since CFD bonds are land-secured, the willingness of property owners to pay their annual special taxes may be affected by reductions in land values.</p> <p>CON: The interest rates on these bonds are generally 50 to 75 basis points higher than bonds secured by the City's General Fund.</p> |
| Special Assessment Districts (AD) | Special Assessment Districts provide mechanisms for cities, counties, and other organizations to construct or acquire public improvements, to apportion the costs through liens against the properties in a designated area that directly benefits from the improvements, and to finance the liens through the issuance of tax-exempt bonds. | Special Assessment Districts have broad powers and can finance a wide range of public facilities including libraries, parks, open space, public administration buildings, police and fire stations, and schools as well as roads, sewer, water, and storm drain maintenance and improvements. | <p>The formation of an AD is subject to a majority ballot by affected property owners within the district. Landowners have the opportunity to return their ballot either in protest or in favor of the formation of an AD. If a majority protest is received, the agency must abandon formation proceedings for a minimum of one year.</p> <p>Each parcel of property within an AD is assessed a portion of the costs of the public improvements and/or services to be financed by the AD, based on the proportion of special and direct benefit from these public improvements and services received by that parcel. Assessment liens are levied at the time of formation of the AD and installment payments are collected along with property taxes on a semi-annual basis.</p> | <p>PRO: AD bonds permit the funding of infrastructure as needed, unlike most pay-as-you-go mechanisms such as development impact fee programs.</p> <p>PRO: AD bonds are land secured obligations, so the City's General Fund and taxing authority are not at risk.</p> <p>PRO: AD bonds are tax-exempt, resulting in interest rates that are lower than conventional financing.</p> <p>PRO: Formation of an AD requires a majority of the ballots returned to be in support of their assessments, rather than the two-thirds requirement under a CFD.</p> <p>CON: All improvements and services funded by an AD must provide special, measurable, local, and direct benefit to the parcels within the AD.</p> |





Table 8-5
Public Debt Financing Mechanisms

| FINANCING MECHANISM | FUNCTION | ELIGIBLE ACTIVITIES | FORMATION AND VOTING REQUIREMENTS | OTHER NOTES |
|--|--|--|--|---|
| Infrastructure Financing Districts (IFD) | Cities and counties can create IFDs to pay for regional-scale public works using tax increment financing in a similar fashion as under redevelopment law. Unlike redevelopment, however, the property in an IFD doesn't have to be blighted. | IFDs can divert property tax increment revenues for 30 years to finance highways, transit, water systems, sewer projects, flood control, child care facilities, libraries, parks, and solid waste facilities. IFDs cannot pay for maintenance, repairs, operating costs, and services. | <p>The City must develop an infrastructure plan, send copies to every landowner, consult with other local governments, and hold a public hearing.</p> <p>Every local agency that will contribute its property tax increment revenue to the IFD must approve the plan. Schools cannot shift their property tax increment revenues to the IFD.</p> <p>Once the other local officials approve, the city or county must still get the voters' approval to:</p> <p>Form the IFD (2/3 voter approval). Issue bonds (2/3 voter approval). Set the IFD's appropriations limit (majority voter approval).</p> <p>When forming an IFD, a "base year" is identified; the ad valorem tax revenues paid on the assessed property value in the base year continue to be passed through to the agencies that receive ad valorem tax revenues.</p> <p>However, the revenues that result from an increase in assessed value above the base year assessed value are called tax increment (TI) revenues, and a portion of these revenues are retained by the IFD. An IFD can then issue bonds secured by the TI revenue it receives, or will receive, because of the continuing increase in assessed value.</p> | <p>PRO: IFDs provide a source of funding without requiring special taxes, assessments, or user fees.</p> <p>PRO: IFD bonds would not be considered City debt. TI revenues from taxing agencies that do not wish to give up any of their revenues can be excluded from the IFD. Likewise, an agency can specify the percentage of its TI revenues to be used.</p> <p>CON: An IFD reduces general fund revenues for participating agencies, and therefore may not be politically or economically viable.</p> <p>PRO/CON: IFDs in which residential development will occur must set aside 20 percent of such units for low- and moderate-income housing.</p> |

Source: David Taussig & Associates, The Planning Center